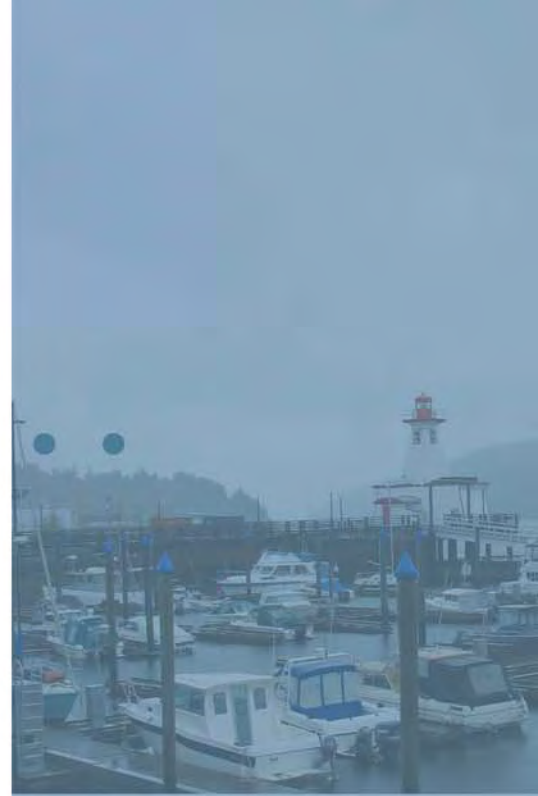


Final Recommendations for Uptown and Waterfront Redevelopment



City of Port Alberni
Port Alberni Port Authority



January 31, 2007



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January 31, 2007

City of Port Alberni,
Port Alberni Port Authority,
C/o Planning Department,
4850 Argyle Street,
Port Alberni, BC
V9Y 1V8

Attention: Mr. Scott Smith, MCIP, City Planner

Re: Final Report – Uptown and Waterfront Redevelopment Strategy

We are very pleased to submit 22 copies of our final report for the Port Alberni Uptown and Waterfront Redevelopment Strategy.

It has been a pleasure to work with City and Port on this project, and to meet various citizens and community leaders that have articulated a strong and caring vision for the community. The consulting team has found the project exciting and stimulating.

We look forward to seeing progress on-the-ground towards implementation of this strategy. Doing so will start to realize the vision shown in the Shoreline Master Plan.

Sincerely,

LANARC CONSULTANTS LTD.,



David Reid, FCSLA, Landscape Architect, Planner
Principal

cc. Tom Lauga, Port Planner
Richard Drdul P.Eng. Community Transportation Planning
Russ Irish, P.Eng. McElhanney Consulting Services

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Map A: Port Alberni Uptown and Waterfront Redevelopment Strategy Plan (separate wall size map)

Appendices

Appendix A: Public Input to Identify Issues
Appendix B: Public Review of Alternatives.....
Appendix C: Contact with First Nations.....
Appendix D: Capital Cost Estimates for Streetscape Improvements
Appendix E: Slide Show Summary of Recommendations
Appendix F: Map A (separate wall size map)

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1. Introduction

A Shoreline Master Plan was created for the City of Port Alberni and the Port Alberni Port Authority (formerly the Port Alberni Harbour Commission) in 1991, and the Plan was updated in 2000.

The 2007 Uptown and Waterfront Redevelopment Plan is focused on strategic recommendations to move forward the implementation of the Shoreline Master Plan, and, in addition, to provide direction for the revitalization of Argyle St. and 3rd Ave. in the Uptown area.

Goals and objectives from the Shoreline Master Plan Review 2000 are still applicable. These include:

- To guide development so that the shoreline becomes a cohesive visual and recreational asset to the community and to visitors.
- To create a series of nodes and linkages along the shoreline, activating its use and improving its accessibility.
- To suggest principles for decision making relative to future development, including guidelines for land use, location of focal points, linkages and specific solutions for critical nodes such as Harbour Quay and Victoria Quay.
- To be complimentary to planning and economic development aspirations of the City, particularly to contribute to economic diversification with emphasis on waterfront tourism wherever possible.
- To continue efforts to ensure that the Port is competitive with other Ports in terms of facilities for efficient and competitive cargo handling systems allowing for the flow of goods through the port and elsewhere across the country.

The terms of reference for the Uptown and Waterfront Redevelopment Strategy 2007 provided for a different level of detail in each of two study areas. Figure 1 provides an outline of the General Study Area as well as the Detailed Study Area.

- For the General Study Area, the Uptown and Waterfront Redevelopment Strategy was intended to review and update the policies in the Shoreline Master Plan Review 2000. In general, the former policies are upheld in this review, and the Uptown and Waterfront study adds detail and an implementation strategy. For areas covered by the Shoreline Master Plan 2000 that are outside the General Study Area outlined in Figure

1, the policies of the Shoreline Master Plan 2000 still apply.

- For the Detailed Study Area that is outlined on Figure 1, the terms of reference called for a conceptual design and cost estimates for proposed improvements on public property. The emphasis of this work was on streetscape improvements to 3rd Ave., Argyle St., and public areas of the Harbour Quay, Harbour Quay Marina, Fishermen's Harbour and adjacent streets and walkways.

2. Undertaking The Strategy

The Uptown and Waterfront Redevelopment Plan focuses on strategic recommendations.

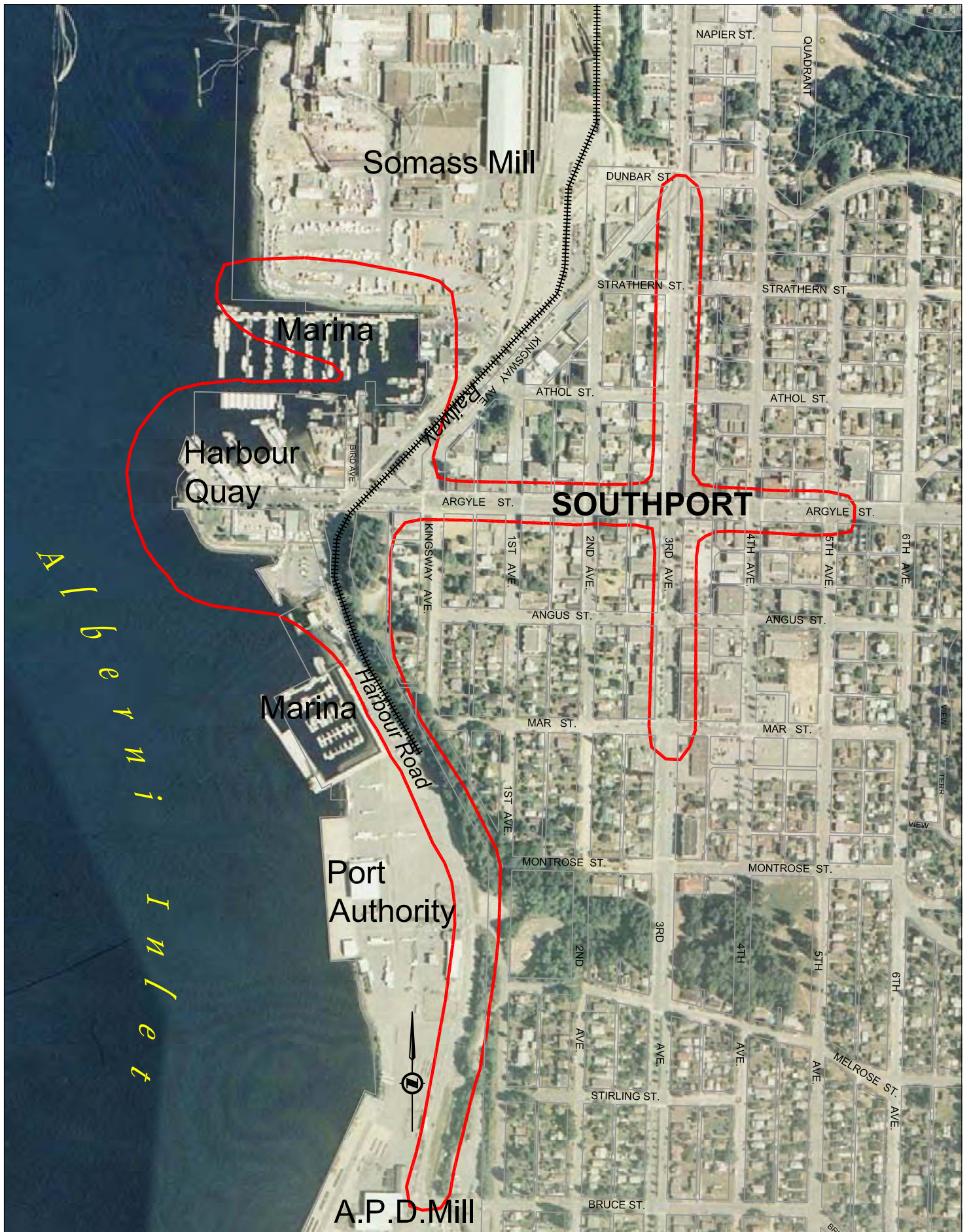
The recommendations were developed based on several steps that included:

- Public Process to Identify Issues
- Detailed Alternatives
- Public Review of Alternatives
- Draft Recommendations Implementation Strategy
- Council and Board Informal Review
- Refinements Based on Input Received
- Final Recommendations and Report

The overall process was undertaken through 2006, with the report presented early in 2007.

To complete the Strategy, the City of Port Alberni and Port of Port Alberni jointly funded the appointment of a multidisciplinary team of planners and engineers. The team, headed by Lanarc Consultants Ltd., included:

- David Reid, FCSLA, Landscape Architect, Planner, Lanarc Consultants Project Manager
- Nigel Gray, BLA, Landscape Architect, Lanarc Consultants, with assistance by Lanarc staff
- Tom Lauga, Port Planner with Lauga & Associates Consulting Ltd.
- Richard Drdul P.Eng. with Richard Drdul Community Transportation Planning
- Russ Irish, P.Eng. with McElhanney Consulting Services Ltd.



STUDY AREA 2

Somass Mill



Harbour Quay

Albion Inlet

Marina

Port Authority

Harbour Road

Harbour Road

SOUTHPORT

DUNBAR ST.

STRATHERN ST.

STRATH

ATHOL ST.

ATHOL

BIRD AVE.

ARGYLE ST.

KINGSWAY AVE.

1ST AVE.

2ND AVE.

3RD AVE.

4TH AVE.

ANGUS ST.

ANGUS

MAR ST.

MAR ST

1ST AVE.

MONTROSE ST.

MONTROSE

2ND

AVE.

3RD

AVE.

4TH

AVE.

3. Public Process

The Public Process

The public process involved six steps:

- On March 30, 2006, an Introduction Meeting was held at City Hall with key leaders in the community.
- On April 20, 2006, a public Issues and Ideas Workshop was held at the Echo Centre.
- Informal meetings were held with City staff, Port staff, and a joint meeting of the Industrial Heritage Society and the Marine Heritage Society was held.
- Meetings were held with each of the Tseshaht & Hupascaseth First Nations.
- On June 28, 2006, a public Alternatives Workshop was held at Echo Centre.
- Draft Recommendations were presented at an informal gathering of City Council, Port Directors and related staff on September 29, 2006.
- After an opportunity for review and input from both the City and the Port, a Final Draft Report was prepared in early 2007, for final review by staff and a public presentation to a meeting of Council and the Port Authority.

The public and staff input have had a substantive impact on the policies and recommendations in this report.

Full public input results are tabulated in Appendix A and B.

4. Alternatives Considered

Two Alternatives

Two alternatives were developed to allow a careful study and discussion about benefits relative to investment. This section records the alternatives reviewed:

- Alternative A: Low to Moderate Public Investment
- Alternative B: High Public Investment

Goals Common to All Alternatives

A set of goals, developed through the public process and in consultation with staff, were common to both alternatives:

1. Improve linkage between waterfront and uptown, up and down Argyle, both in terms of views and access.
2. Showcase maritime and industrial heritage.
3. Showcase environmental sustainability.
4. Improve appearance and function of 3rd Ave. retail district.
5. Improve land use and social conditions of 3rd Ave. retail district.
6. Ease pedestrian crossing of Harbour Road at Argyle
7. Balance community uses and economic development potential for the Harbour View property.
8. Plan redevelopment of the Water Street Wharf / Esso Property area to improve public waterfront access and address economic development.
9. Provide infrastructure for Cruise Ship visits to Port Alberni Terminals, and adapt the Lighthouse Area to suit.
10. Identify redevelopment and waterfront access potential for Plywood Site if covenant restricting residential use IS NOT removed.
11. Identify redevelopment and waterfront access potential for Plywood Site if covenant restricting residential use IS removed.
12. Include First Nations as respected partners in waterfront improvements.
13. Allow gradual redevelopment of the Harbour Quay and Fishermen's Harbour area.

14. Increase waterfront public access.
15. Promote jobs and diverse economic opportunities
(both for the Port and for the Uptown area).

Comparison of Alternatives

Table 1 provides a summary of the key differentiating features of the two alternatives.

Figure 2 is a slide show that summarizes the alternatives.

Table 1 Comparison of Alternatives

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
1.	Improve linkage between waterfront and uptown, up and down Argyle, both in terms of views and access.	<ul style="list-style-type: none"> Maintain 4 travelled lanes plus angle parking both sides. Install curb bulges at intersections and new pedestrian crossing provisions. Replace curbs and install wider sidewalks, include street trees between parking and sidewalks, by slightly reducing parking size. Improve BC Transit bus service between Harbour Quay and 3rd Ave. Encourage redevelopment of underutilized City properties along Argyle to mixed retail/residential/office and parking uses that will create a continuous pedestrian-oriented experience at street level along Argyle. 	<ul style="list-style-type: none"> Reduce from 4 travelled lanes to 2 travelled lanes plus angle parking both sides. Install curb bulges at intersections and new pedestrian crossing provisions. Add a central median of flowers between intersections - no trees in median to allow the mountain view. Lanes become right in / right out only or have limited crossing provisions at median. Replace curbs and install wider sidewalks, include street trees between parking and sidewalks. Provide a seasonal / special occasion rubber-tired shuttle between Harbour Quay and 3rd Ave. – could be a ‘theme’ vehicle e.g. ‘crummy’. Encourage redevelopment of underutilized City properties along Argyle to mixed retail/residential/office and parking uses that will create a continuous pedestrian-oriented experience at street level along Argyle.
2.	Showcase maritime and industrial heritage	<ul style="list-style-type: none"> Install maritime and industrial artifacts where sight distance allows on intersection curb bulges. Arrange for guided tours of local industries and the waterfront. 	<ul style="list-style-type: none"> Create professional DVDs and videos that are virtual tours of local industries, people and related markets. Show these in unmanned kiosks along the street to create a series of ‘attractions’ spaced optimally. Also make versions for sale in local stores. Incorporate the video screens into customized pieces of art spaced along Argyle Street.
3.	Showcase environmental sustainability	<ul style="list-style-type: none"> Increase tree planting. Install interpretive signage to highlight the ‘cleanup’ of local industries over the last 25 years. 	<ul style="list-style-type: none"> Use separated stormwater runoff to feed ‘infiltration swales and rain gardens’ in the Argyle medians or boulevards. ‘Celebrate the Rain’.

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
		<ul style="list-style-type: none"> Use 'Green Shore' techniques as a part of any waterfront redevelopment. 	<ul style="list-style-type: none"> Consolidate and increase the amount of green space and high-value uses on the waterfront, with consideration of alternate locations for lower value uses like surface parking. Encourage improvement to waterfront water quality during redevelopment.
4.	Improve appearance and function of 3 rd Ave. retail district.	<ul style="list-style-type: none"> Add planted median to painted line areas north of Argyle. Roadside improvements, including replace lighting with new decorative fixtures, repair crosswalk brick paving and localized lifted paving, renew shrub and flower planting in planters, improve street signage and street furniture. Screen off-street parking areas with planted hedge or decorative fence. 	<ul style="list-style-type: none"> Replace existing wood bench cap on stone planters with concrete (or stone) cap and anti-skating devices. Encourage a business improvement area (BIA) to start programs to increase marketing and improvement of private properties. Add planted median to painted line areas north of Argyle (same as Alt. A). Roadside improvements (same as Alt. A), including replace lighting with new decorative fixtures, repair crosswalk brick paving and localized lifted paving, renew shrub and flower planting in planters, improve street signage and street furniture.
5.	Improve land use and social conditions of 3 rd Ave. retail district.	<ul style="list-style-type: none"> Encourage a street merchant outreach program to provide information and alternatives to those in need. Increase enforcement of existing or new bylaws. 	<ul style="list-style-type: none"> Review land use policies to encourage market and/or social housing and addiction treatment services as part of the redevelopment of the Beaufort lands (e.g. allow work/live units along the street frontage, and public parking behind under a office –like treatment centre with housing above). Encourage outreach and enforcement (same as Alt A.)
6.	Ease pedestrian crossing of Harbour Road at Argyle	<ul style="list-style-type: none"> Reconfigure the access to Bird Ave. to increase pedestrian comfort and safety. Install improved materials and curb bulges to ease pedestrian crossing of the railway tracks, 	<ul style="list-style-type: none"> Investigate a land exchange or sale to re-orient Bird Ave. away from this intersection, and encourage private redevelopment of underused properties in this area.

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
		<p>while recognizing turning movement requirements of large trucks.</p> <ul style="list-style-type: none"> Improve pedestrian access through the Railway Station property. 	<ul style="list-style-type: none"> (same as Alt A) Install improved materials and curb bulges to ease pedestrian crossing of the railway tracks, while recognizing turning movement requirements of large trucks. (same as Alt A) Improve pedestrian access through the Railway Station property.
7.	Balance community uses and economic development potential for the Harbour View property.	<ul style="list-style-type: none"> Retain the 'core' of existing fir trees at the hilltop. Consider limited development of the north edge of the property, for retail along Argyle and residential above (may impact up to 4 trees). Parking for this may include part of the existing surface parking on the site's east side. Consider limited development of the south edge of the property, for residential use with parking under (may impact up to 3 trees). Improve the central treed open space, parking and street frontage area as a high-amenity public area and parking. Some of the existing parking would be dedicated to condo use. 	<ul style="list-style-type: none"> Retain the 'core' of existing fir trees at the hilltop, as well as most of the grassy slope down to Argyle. Provide 'special event' and access improvements between Argyle and the treed area e.g. bandstand, gazebo, stair, seating slope, etc. Consider limited development of the south edge of the property, for residential use with parking under (may impact up to 3 trees). Extend this housing development on concrete deck over part of Harbour Road and the railway, with a pedestrian spine / fire access gently sloping down to join the waterfront trail at Harbour Quay, with Harbour Road and parking under. West of Harbour Road, provide an enclosed 'heritage rail work area' under the overhead deck, and allow continuous trackage so that heritage trains can be displayed along the existing tracks to the south of the 'work area' Develop the central and northern treed open space, parking and verge area as a high-amenity public park and parking. Some of the existing parking would be dedicated to condo use.
8.	Plan redevelopment of the Water Street Wharf / Esso Property area to improve public waterfront access and address economic development.	<ul style="list-style-type: none"> Remove and perhaps replace Water Street Wharf in a new configuration, approximately at its current location. Consider provisions for tie-ups of visiting vessels at both wharfage and floats. Include a waterfront walk that crosses behind Water Street Wharf to extend along the Esso 	<ul style="list-style-type: none"> Develop a new waterfront walk on fill / piles that connects Harbour Quay along the front of the proposed housing, through the old Esso property above the existing wall and connects to the existing Marina walkways. Improve the waterfront of the Esso property for parks use.

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
		<p>property to join to existing walks at the Harbour Quay Marina. Improve the waterfront of the Esso property for public use.</p> <ul style="list-style-type: none"> Boat ramp in this area is abandoned. 	<ul style="list-style-type: none"> Encourage private development of a new marina to replace Water Street Wharf and the water between it and the existing Harbourfront Marina. Part of the moorage at this marina would be made available to the upland condo owners. Outer floats at this marina would be designed to moor larger boats, and for special event moorage. Boat ramp moves to the Plywood site.
9.	Provide infrastructure for Cruise Ship visits to Port Alberni Terminals, and adapt the Lighthouse Area to suit.	<ul style="list-style-type: none"> Provide an attractive pedestrian gate system near the Maritime Gallery to manage customs clearance, and install attractive outdoor displays and landscape to welcome Cruise Ship pedestrians onto the waterfront walkway system. Consider a new 'customs' facility near the Maritime Gallery, if demand warrants (may be a temporary structure initially). Adapt existing floats in the Harbour Quay Marina to accept 'tender' transport from anchored Cruise ships. Use the existing Marina parking for taxi or small vehicle pickup. Allow (with inspection and permit) buses and large tour vehicles to use the existing secure terminal loading area to pickup and drop off cruise ship passengers. 	<ul style="list-style-type: none"> (Same as Alt A) Provide an attractive pedestrian gate system near the Maritime Gallery to manage customs clearance, and install attractive outdoor displays and landscape to welcome Cruise Ship pedestrians onto the waterfront walkway system. (Same as Alt A) Consider a new 'customs' facility near the Maritime Gallery, if demand warrants. (may be a temporary structure initially) (Same as Alt A) Adapt existing floats in the Harbour Quay Marina to accept 'tender' transport from anchored Cruise ships. Extend the existing Harbour Quay Marina Parking Area to the south, into a corner of the existing Terminal area, to provide additional parking as well as turning radius and parking for buses and large vehicles. Create attractive security fencing between this transit exchange and the terminal area.
10.	Identify redevelopment and waterfront access potential for Plywood Site if covenant restricting residential use is NOT removed.	<ul style="list-style-type: none"> Assemble, market and sell land / water lot for a new heavy industry along this waterfront e.g. a Specialty Sawmill. Could include some waterfront fill to expand the width of the site, or pile-based structure. 	<ul style="list-style-type: none"> Assemble, market and sell land for a 'ship storage and service' cluster along the northern section of this waterfront. Would include covered, stacked boat storage, marine lift, repair facilities, service facilities, and potentially

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
		<ul style="list-style-type: none"> Waterfront would be log storage and/or industrial marine access. Access to site would be Plywood Drive and/or through APD. Rail access may be possible through APD if tracks were re-established. Upper terrace could be employee parking / site office and/or product storage. Public access to a waterfront viewpoint or surplus waterfront lands could be negotiated as part of the land sale. 	<p>boat sales facilities.</p> <ul style="list-style-type: none"> Develop public parking and a public seawalk along the central and southern edges of the property. Redevelop the 'pier' to act as equivalent to Water Street Wharf. Improve truck and float provisions for small craft (FN) fishery landing. Provide floats and provisions for visiting large and small craft. Consider leasing pier or adjacent space for a waterfront restaurant or pub, or dive shop. Encourage (or partner with) local First Nations to develop a new small craft marina in this area – either north or south of the pier. Use the upper terrace for parking and/or boat storage. Some borrow of fill might come from this terrace.
11.	Identify redevelopment and waterfront access potential for Plywood Site if covenant restricting residential use IS removed.	<ul style="list-style-type: none"> (same as above) 	<ul style="list-style-type: none"> (same as above) Market and sell land / water lot for a 'ship storage and service' cluster along the northern section of this waterfront. Would include covered, stacked boat storage, marine lift, repair facilities, service facilities, and potentially boat sales facilities. (same as above) Develop public parking and a public seawalk along the central and southern edges of the property. Market condo stepping down over the shoreline slopes, to a lower level at the south end of the site. Habitable areas should be above 10m for tsunami protection. (same as above) Consider leasing pier or adjacent space for a waterfront restaurant or pub, or dive shop. Market water lots for a private marina development – serving both the upland condos and the public. Could be also marina

#	Goals	Alternative A: Low Investment	Alternative B: High Investment
			development in association with First Nations.
12.	Include First Nations as respected partners in waterfront improvements.	<ul style="list-style-type: none"> ▪ Consultation throughout the process. 	<ul style="list-style-type: none"> ▪ Consultation throughout the process. ▪ Creation of a 'joint venture' partnership for key projects where there is common interest and capability.
13.	Allow gradual redevelopment of the Harbour Quay and Fishermen's Harbour area.	<ul style="list-style-type: none"> ▪ Limit public sector involvement, but encourage private redevelopment as market allows. ▪ Provided that continuous waterfront public access remains, and replacement of playground elsewhere on the waterfront, and movement of farmer's market to 3rd Ave. area, consider limited private development of existing playground and parking area at Harbour Quay to a higher use. 	<ul style="list-style-type: none"> ▪ Provide public sector planning to support land exchange or purchase to create more viable parcels for private redevelopment as market allows. For example, consider how the parcels between the Somass Mill and the Lady Rose could be reconfigured to best support gradual private sector improvements and complementary marine-oriented uses. ▪ Pursue agreement with Somass Mill to allow a public boardwalk and limited associated public parking along the south edge of its property (fronting the Fisherman's Harbour).
14.	Increase waterfront public access.		
15.	Promote jobs and diverse economic opportunities(both for the Port and for the Uptown area).		

Figure 2: Slide Show Summary of Alternatives

Alternatives Workshop

June 28, 2006

Presentation 7:00 to 8:00

Small Group Ideas Workshop: 7:30 to 8:30

Reporting 8:30 to 9:00



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



The Alternatives

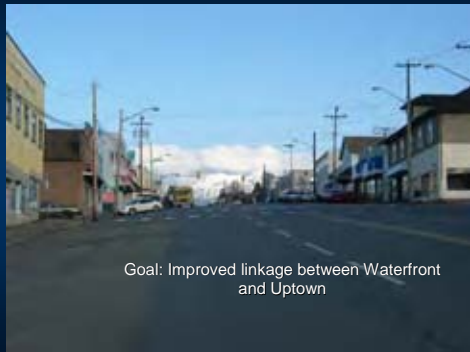
Option A: Low to Moderate Public Investment

Option B: High Public Investment

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



Goal: Improved linkage between Waterfront and Uptown

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area

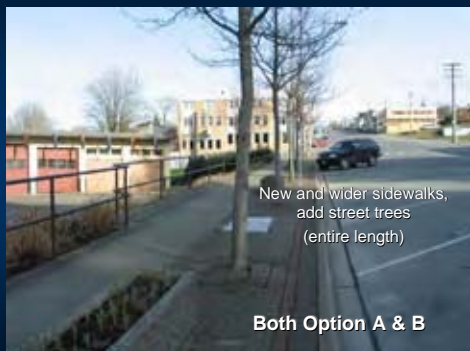


Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



New and wider sidewalks,
add street trees
(entire length)

Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



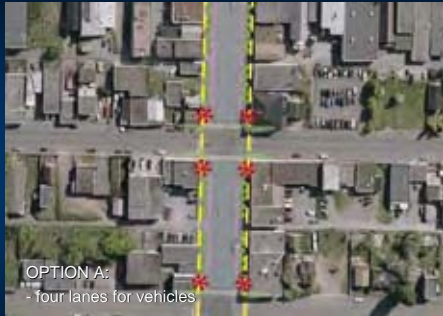
Encourage redevelopment of
vacant city properties - to
mixed retail / office /
residential

Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area

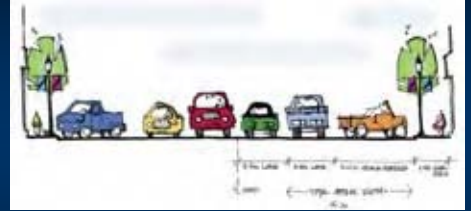


OPTION A:
- four lanes for vehicles

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



OPTION A:
- four lanes for vehicles

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area

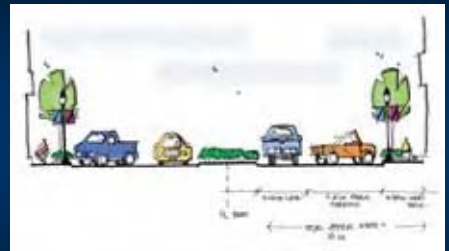


OPTION B:
- two lanes for vehicles
- central landscape median

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



OPTION B:
- two lanes for vehicles
- central landscape median

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Argyle Area



OPTION A:
- improved BC Transit
service

OPTION B:
- seasonal / special occasion
rubber tired shuttle e.g. a
crummy?

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Maritime and Industrial Heritage

- Goals:
- showcase an 'adaptable' local theme
 - create a series of 'attractions' to pull tourists up Argyle to 3rd Ave.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Maritime and Industrial Heritage

OPTION A:

- install artifacts on the sidewalk where sight distance allows at intersections.
- arrange for guided tours of local industries and the waterfront

OPTION B:

- create professional DVDs and videos that are virtual tours of local industries.
- show these in unmanned kiosks on the street, incorporated in custom art pieces.
- make DVDs available for sale in local stores.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Environmental Sustainability

OPTION A:

- install interpretive signage to highlight 'cleanup' of local industries over the last 25 years.
- use 'green shore' techniques as a part of waterfront development.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Environmental Sustainability



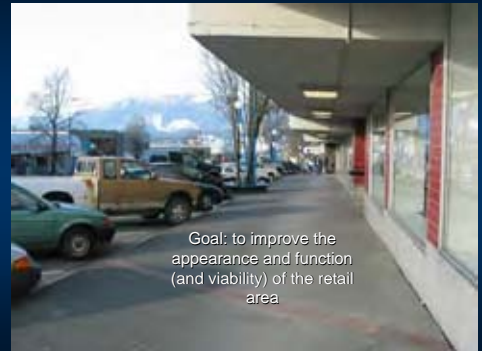
OPTION B:

- 'Celebrate the Rain' with median or boulevard Rain Gardens along Argyle.
- consolidate and increase waterfront green space and high value uses.
- encourage waterfront water quality improvements during redevelopment

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area



Goal: to improve the appearance and function (and viability) of the retail area

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area



Planted median to painted line areas on 3rd north of Argyle

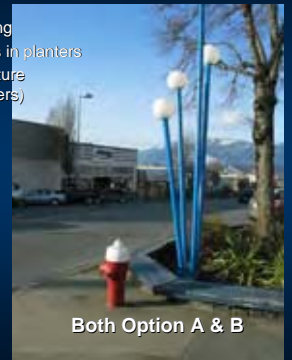
Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

- new decorative lighting
- renew shrub / flowers in planters
- add better street furniture (benches, litter containers)



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

- improve street signage



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

- repair crosswalk brick paving



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

- repair sidewalks where required



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area



OPTION A:

- screen off-street parking areas with planted hedge or decorative fence.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

OPTION B:

- replace existing wood bench cap with concrete or stone cap and anti-skating devices.
- encourage formation of a Business Improvement Area (BIA) for marketing and improvement of private properties.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area

Goal: to improve land use
and social conditions



- encourage a street merchant outreach program to provide information and alternatives to those in need.

Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



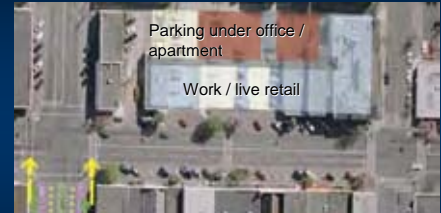
3rd Ave. Area

OPTION A:
- increase enforcement of
existing or new bylaws.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area



OPTION B:
- encourage market and/or social housing and
addiction treatment services as a part of
redevelopment of the Beaufort lands.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



3rd Ave. Area



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing



Goal: to increase pedestrian
comfort and safety, and
encourage walking to Argyle
and 3rd Ave. retail areas.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing



Goal: to maintain truck
movements, and allow for the
future Harbour Road
upgrade.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing



- install improved materials and
curb bulges to ease pedestrian
crossing of the railway tracks.

Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing

- install pedestrian-activated signals.



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing

- improve pedestrian access through the railway station property.



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing



OPTION A:

- reconfigure the access to Bird Ave. (e.g. a driveway across a sidewalk island)

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Road Crossing



OPTION B:

- investigate a land exchange or sale to re-orient Bird Ave away from the intersection.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Sea Cadet Site

Goal: to balance community uses and economic development potential for this water-view site.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Sea Cadet Site

- retain the core of existing fir trees at the hilltop.
- improve the central treed area, parking and street frontage as a high amenity public area and parking.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Both Option A & B

Sea Cadet Site



OPTION A:

- limited development on the north edge of the property – affecting up to 4 trees (retail with residential over)
- limited residential development on the south edge of the property – affecting up to 3 trees

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Sea Cadet Site

OPTION B:

- special event and access improvements on the north side – bandstand / gazebo, stair, seating slope, etc.
- housing development that extends from the south corner of the Sea Cadet site on slab over the railway / Harbour Road, with a promenade to join Harbour Quay near Water St. Wharf.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Sea Cadet Site



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

Goals:

- to achieve replacement of the Water Street Wharf and boat ramp, as well as adaptive use of the environmentally constrained 'Esso' site.



- to improve public waterfront access
- to increase port economic viability

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

- add a waterfront walk to connect through the Esso site.
- improve the old Esso waterfront for public use.



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area



- boat ramp moves – to either plywood site or Fishermen's Wharf area.

Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

OPTION A:

- remove and replace Water St. Wharf in a new configuration – allow for pocket size cruise ships at wharfage and floats.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

OPTION A plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

OPTION B:

- Wharf moves to Plywood Site
- waterfront walk fronts retail with parking behind and housing above.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

OPTION B:

- new marina floats – both for condo owner boats and for special events / pocket cruises / tall ships, etc.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Water Street Wharf Area

OPTION B Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Small Cruise Ship Infrastructure



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



"Pocket cruise" ships like the Sea Lion attract passengers who want to learn about the environment, wildlife and natural scenery. To be a gateway to the west coast of Vancouver Island, Port Alberni may need berthing spaces for such vessels in addition to the MV Lady Rose or MV Frances Barkley.

Large Cruise Ship Infrastructure



This proposal for a cruise berth in Juneau Alaska shows the important elements for serving cruise passengers – except, perhaps, current security needs.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure

- provide attractive pedestrian gate systems for customs clearance needs.
- install landscape and interpretive displays to the welcoming and customs holding area.
- consider a new customs shelter if demand warrants (may be temporary).



Both Option A & B

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure

OPTION A:

- no new parking – use existing at Harbour Quay Marina.
- allow tour buses, after clearance, to enter the secure port terminal to pick up cruise passengers.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure

OPTION A Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure

OPTION B

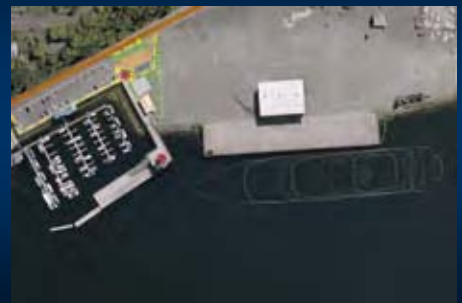
- new parking for buses, taxis, etc. extending south from existing.
- attractive fencing/landscape between this transit exchange and the existing port terminal.

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure

OPTION B Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Large Cruise Ship Infrastructure



The White Pass Railway in Skagway Alaska is the most important single factor in attracting cruise passengers to this port. The railway attracts passengers and is an important part of the revenues of the cruise lines. Perhaps, Port Alberni has the same potential?

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Plywood Site

Goals:

- to encourage economic development and jobs for PA.
- to increase public waterfront access.
- to co-operate with neighbours – industrial and First Nations.
- to make better use of this under-utilized resource.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Plywood Site

Option A:

- assemble, market and sell land for new heavy industry
- may involve co-operation with A.P.D. and overlap with the A.P.D. site.
- upper, waterfront and marine areas would all be considered to encourage industry investment.
- at the time of sale, negotiations could occur about inclusion of waterfront viewpoints and localized public access.

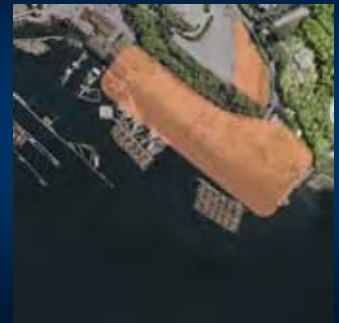


Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Plywood Site

Option A Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Plywood Site

Option B:

- north portion of site is a boat -service oriented use – two options.
- south portion of waterfront is public uses and a replacement for the Water St. Wharf and boat ramp.
- upper terraces may be regraded for condo or condo-tel use, if covenant is removed.

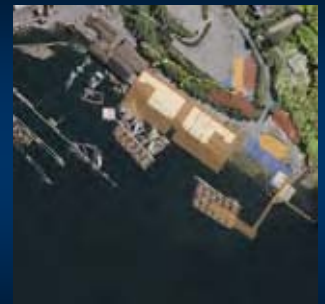


Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Plywood Site



Option B Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept




Boat storage buildings offer attractive protected storage for boats up to about 35 feet long. Boat owner maintenance costs are reduced and the environment is protected from various emissions such as fuel, oil and anti-fouling boat paints.





For seniors, this type of storage provides a kind of "boat valet" service. For condominium owners and part-time residents this type of storage is particularly attractive. Scarce water frontage is conserved.


Port Alberni Uptown & Waterfront Redevelopment Concept



Point Hope Shipyard plan in Victoria is an example of a mix of residential and port-industrial development. The shipyard in the middle is like an anchor tenant in an "industrial shopping centre". It is a kind of incubator for small businesses.




Port Alberni Uptown & Waterfront Redevelopment Concept




Plywood Site

Plywood Site





Option B: - waterfront marina may be either north (restricted by water depth) or south of the wharf (with F.N. co-operation).

Port Alberni Uptown & Waterfront Redevelopment Concept




Plywood Site


Option B: Waterfront commercial tours and small craft rentals, dive shop?

Port Alberni Uptown & Waterfront Redevelopment Concept




Plywood Site



Option B: Or a Floating Restaurant or Marine Pub

Port Alberni Uptown & Waterfront Redevelopment Concept



Harbour Quay / Fishermen's Wharf Area

Goal: Gradual Improvement



Port Alberni Uptown & Waterfront Redevelopment Concept



Harbour Quay / Fishermen's Wharf Area

Option A:

- look to the private sector for improvements.
- IF playground was replaced (Esso site) and Farmer's Market moved to 3rd Ave., could their existing sites be considered for retail with residential above?



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option A:

- sites around the Fishermen's Wharf would be encouraged to redevelop gradually, as market allows.



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option A: Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option B:

- public sector to more aggressively encourage private sector.
- provide public sector planning to support land exchange or purchase to create more viable parcels for private redevelopment.



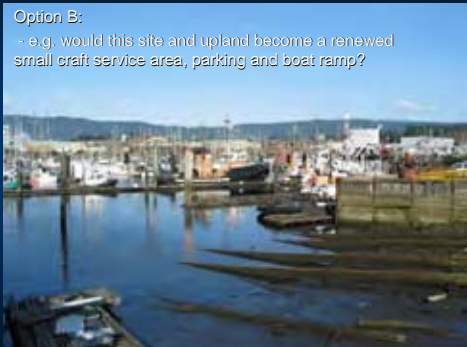
Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option B:

- e.g. would this site and upland become a renewed small craft service area, parking and boat ramp?



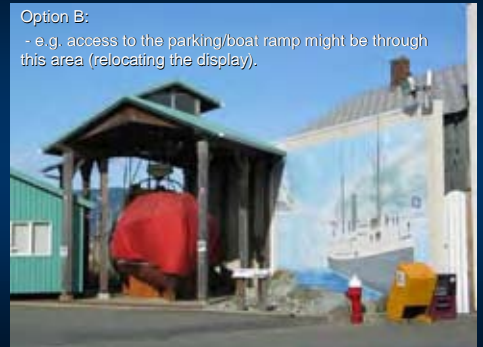
Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option B:

- e.g. access to the parking/boat ramp might be through this area (relocating the display).



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option B:

- e.g. better parking might support the 'marine pub'



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area



Option B: might find a sheltered location for small boat offloading and sales?

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area

Option B Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area



Land-based Boardwalk and parking on south side of Somass Mill site to a new dockhead?

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf Area



Beyond the proposed dockhead at Somass Mill, a pier extension

Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



Harbour Quay / Fishermen's Wharf

to an overlook of the harbour activity?



Port Alberni
Uptown &
Waterfront
Redevelopment
Concept



5. Public Review of Alternatives

Public Review Meeting

A Public Review Meeting of Alternatives was held at Echo Centre on June 28, 2006. Approximately 80 interested people attended. After the presentation there was a small group workshop where respondents were asked to fill out a response form together in small groups – leading to identification of consensus points and remaining issues about the alternatives.

Results of the Response Forms

Appendix B provides the summary of responses, as well as written comments received.

The relatively small sample of 34 response forms is not statistically valid.

Table 2 provides a summary of the key elements of the Alternatives that seemed to gain support from more than 50% of the respondents.

There are some elements in the response form that generated less clear direction from respondents, including:

- What improvements to land use and social conditions on 3rd Ave. do you support?
- What changes to the Water Street Wharf / Esso Property area do you prefer (clear majority favored replacing the Wharf with a public waterfront walkway and an adjacent marina (privately funded)).
- Do you support the 'Large Cruise Ship Infrastructure' that is proposed? We believe that respondents were influenced by the Consultants presentation, which cautioned against premature large expenditures on Cruise Ship Infrastructure. This view is reflected in the mixed level of support for improvements at this time. However, a clear majority favored Option B, which represented a higher investment than Option A, if and when investment is made.
- What land uses at the 'Plywood' site south of APD Mill do you support? Respondents were split here too, and no alternative gained more than 50%. However, respondents favored mixed use of residential /marine commercial over heavy industry.

Table 2 Summary of Responses >50% in Support

Concept or Amenity	% Support if >50%
What improved linkage between waterfront and Uptown do you support, on Argyle between 4th Ave. and Kingsway?	
▪ Retain existing angle parking	▪ 53%
▪ Add curb bulges at intersections	▪ 56%
▪ New and wider sidewalks (widths vary)	▪ 56%
▪ Underground wires	▪ 74%
▪ Encourage redevelopment of vacant City properties to mixed retail / office / residential	▪ 76%
▪ Provide 2 traveled lanes of traffic, plus central landscaped median, and seasonal / special occasion rubber tired shuttle	▪ 62%
How should maritime and industrial heritage be showcased?	
▪ Install maritime and heritage artifacts where sight distance allows on intersection curb bulges.	▪ 74%
▪ Arrange for guided tours of local industries on the waterfront	▪ 53%
What actions to showcase environmental sustainability do you support?	
▪ Increase tree planting	▪ 65%
▪ Use 'Green Shore' techniques as part of any waterfront redevelopment.	▪ 53%
▪ Encourage improvement to waterfront water quality during redevelopment.	▪ 56%
What improvements to appearance and function of 3rd Ave. do you support?	
▪ Planted median to painted areas north of Argyle	▪ 56%
▪ Replace lighting with new decorative fixtures	▪ 68%
▪ Repair crosswalk brick paving and sidewalks	▪ 59%
▪ Renew shrub and flower plantings in planters	▪ 65%
▪ Improve street signage	▪ 71%
▪ Add better street furniture (benches, litter containers)	▪ 68%

Table 2 Summary of Responses >50% in Support (cont'd)

Concept or Amenity	% Support if >50%
How do you think pedestrian crossing of Harbour Road at Argyle could be improved?	
<ul style="list-style-type: none"> Install improved materials and curb bulges to ease pedestrian crossing of the railway tracks, while recognizing turning movement requirements of large trucks 	<ul style="list-style-type: none"> 53%
<ul style="list-style-type: none"> Improve pedestrian access through the Railway Station property. 	<ul style="list-style-type: none"> 59%
How can we balance community uses and economic development potential for the Harbour View property?	
<ul style="list-style-type: none"> Retain the 'core' of existing fir trees at the hilltop. 	<ul style="list-style-type: none"> 56%
<ul style="list-style-type: none"> Provide 'special event' and access improvements between Argyle and the treed area e.g. bandstand, gazebo, stair, seating slope, etc. Consider limited development of the south edge of the property, for residential use with parking under (may impact up to 3 trees). Extend this housing development on concrete deck over part of Harbour Road and the railway, with a pedestrian spine / fire access gently sloping down to join the waterfront trail at Harbour Quay, with Harbour Road and parking under. West of Harbour Road, provide an enclosed 'heritage rail work area' under the overhead deck, and allow continuous trackage so that heritage trains can be displayed along the existing tracks to the south of the 'work area'. 	<ul style="list-style-type: none"> 56%
What gradual redevelopment of the Harbour Quay and Fishermen's Wharf area is appropriate?	
<ul style="list-style-type: none"> Pursue agreement with Somass Mill to allow a public boardwalk and limited associated public parking along the south edge of its property (fronting the Fishermen's Harbour), 	<ul style="list-style-type: none"> 59%

Informal Input by City Council, Port Authority Board, and Staff

After a careful review of the public input, the consultants developed a set of Draft Recommendations. These were presented to an informal meeting of City Council, the Port Authority Board, and related staffs on September 29, 2006. The public input received was presented at the same session.

In general, the recommendations tended to reflect the public input received. With a few exceptions noted below, the comments of staff, Council and the Port Authority Board seemed to be generally consistent with the majority of comments from the public.

A process of more detailed review by staff, Council and the Board proceeded through the fall of 2007. The Consultants received written comments from both.

Input from City and Port officials that refined the draft recommendations included:

- A recommendation from the City Engineer that Argyle not have a median included in its streetscape, due to concerns about cost, snow clearing of the street, and median maintenance.
- A request by Council to scale out and be sure of the potential impact of partial development on the core of trees at the Sea Cadet (Harbour Village) site. A scaled footprint of a potential development area was laid out on the site, and it was determined that up to 10 fir trees would be impacted by the development. The trees involved are at the south edge of the grove, and their removal would not impact the viability of the remainder of the grove, which would save the great majority of the trees there.
- A request for phasing of improvements on Argyle and 3rd Ave has been made, to help spread out the cost.
- The Port Authority and the City both support the concept of aggressive marketing of the Plywood site for industrial redevelopment. The 'back-up plan' of mixed use / residential development would be kept open if industrial redevelopment was not forthcoming in a reasonable timeframe.
- The Port expects that a Harbour Shuttle would not be financially viable, and therefore this recommendation is altered to allow this if feasible, but to not imply any City or Port subsidy.

In most cases, the written comments refined details of the draft recommendations, but did not vary the general intent.

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6. Recommended Strategy

Key Principles

Four Principles that underpin the Port Alberni Uptown and Waterfront Redevelopment Strategy are:

- Industrial reinvestment and related jobs are important to population and economic stability in Port Alberni and the region.
- At the same time, residential and tourism development within walking distance (400m) is important to long-term success of the Uptown business district.
- The City and Port need to carefully balance their image as friendly to both industrial and residential/tourism investment.
- Both the City and the Port are financially constrained. Implementation will rely on small-scale public investments that facilitate large-scale private investments.

The recommendations 'blend' the elements of the two alternatives that were considered, looking for best overall performance at lowest cost. The recommendations are influenced by the public input received, as summarized in the preceding section and listed in Appendix B.

The following pages outline a set of recommendations that are listed in a general order of priority to start. Many of these actions will start immediately, with overlapping timelines.

Adaptive Management Strategy

Recommendations:

1. The City and Port will adopt an 'Adaptive Management' strategy to manage the Uptown and Waterfront Redevelopment.

Rationale:

Adaptive Management is a planning approach that allows for frequent re-evaluation of programs in the face of new information. It allows decisions to be fine-tuned from year to year, and for strategic 'changes in timing' in response to changes in civic finance, markets and population growth.

For example, private sector interest in industrial or residential development will be difficult to pre-determine, and will vary over time. The Strategy provides recommendations to facilitate opportunities for private sector investment, but also provides 'backup' recommendations to follow in cases where private sector interest is insufficient.

Priority Projects

Recommendations:

2. The key projects in general order of priority are:

- Argyle Infill Development
- Harbour View Village
- Industrial Re-Investment
- Fishermen's Harbour Improvements
- Streetscape Improvements
- Uptown Village
- Harbour Quay Improvements
- Cruise Ship Infrastructure
- Harbour Shuttle

Rationale:

- early projects should generate some cash for the City and Port, which can be used as partnership capital funding of later projects.
- If there are projects where partners are currently willing or able to proceed, those projects should start soon.
- Senior government funding programs have stated objectives, and their priorities may have an influence on project priorities.
- Project priorities will be influenced by the private sector market – whether residential, commercial or industrial.
- Later projects may rely on actions by outside parties (like Cruise Ship lines), and their priority may change as decisions are made by these outside groups.

Argyle Infill Development

Recommendations:

3. The City should proceed to market its existing properties along Argyle Street for residential over street-oriented retail/service.



Fig. 3. Argyle Infill

Rationale:

- There are existing proposals to purchase City property at the southeast corner of Argyle and

Kingsway, for a mixed retail and residential development. This Plan is highly supportive of that development.

- There is potential for two similar developments on City lands along Argyle to the east of Kingsway. Development in mixed use commercial / residential on these two properties would increase the continuity of the retail street wall, which would be helpful in encouraging people to walk on Argyle Street between Harbour Quay and 3rd Ave.
- Revenue to the City from sale of these lands would provide seed capital for other projects in this Strategy, including proposed capital improvements on Argyle Street.

Harbour View Village

Recommendations:

4. The City and Port, with related landowners, should co-ordinate and market a comprehensive development extending from Water St. Wharf, in air rights over Harbour Road, and onto part of the Harbour View property. Actual development of the property would be by the private sector.
 - Refer to the Implementation Steps section.
5. The comprehensive development by the private sector should include:
 - Retention of the majority of existing trees at the Harbour View city property, and limited public parking and park improvements in this area. The public portion of the site would be retained in public ownership and management after the improvements are made.
 - Development of approximately 200 units of residential units (maximum 4 storey height).
 - Creation of a public promenade that extends from the waterfront, over Harbour Road, to link to the waterfront walkway. This promenade would also serve as fire lane for the development. A tie-in to the foot of Argyle at Harbour Quay would be required.
 - Installation, as a part of the private investment, of a marina that includes rental space for private yachts as well as public floats for temporary tie-up of large and small tour boats and for public displays (e.g. tall ships). The

outside floats might also be used by floatplanes during fair weather.

- Creation of a 'boutique' hotel and small commercial building that includes services to manage and support the marina, complementary to existing businesses at Harbour Quay.
- It is expected that the hotel and residential units would be second storey or higher to reduce risks associated with tsunami events. Ground floor units would be commercial or other space that is not sleeping accommodation.
- A continuous waterfront walkway, created as a part of the development, would extend from the existing Harbour Quay walkways to join to existing walkways at Harbour Quay marina.

The 'Esso' property would be redeveloped as a public open space, with pedestrian access to the water's edge, a pocket 'beach', and walkway ramps providing access to the upper level public promenade. No residential development would occur on this property.

- Parking standards for the development would be set at approximately 1 stall per residential unit, plus provision of 'co-op' cars funded initially by the developer.
- The concept plans show provision in the parking garage to provide a route for a marine lift to transport recreational boats from the Harbour Quay area to a launch point that crosses the waterfront walkway. Gating provisions could be made to allow the waterfront walkway to be temporarily closed while a boat is being transported across it. Alternate walkway routes around the gates could be provided. However, it is likely that this location for a Marine Lift will not be supported by adjacent users or the Port, and if that is the case, the route shown could be used either for parking or for service vehicle (truck) access to support the commercial uses, and for emergency vehicle access.
- Along Harbour Road, under the development, passage would be provided to meet Ministry of Transport standards for two lanes of traffic including large forestry trucks, the railway, as well as cycle and sidewalk provisions.



Fig. 4. Harbour View Village

Ground Level

Roof Level

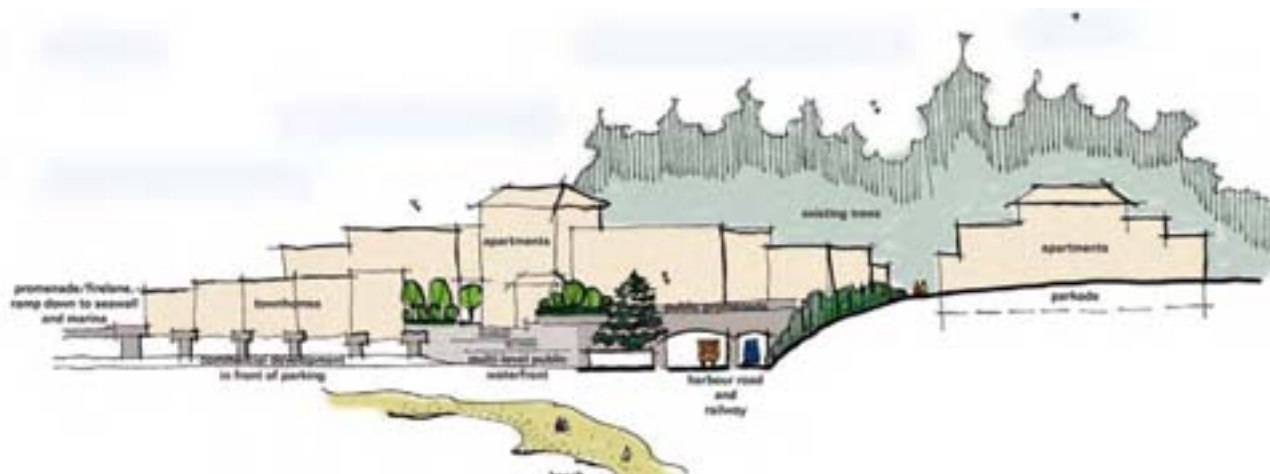


Fig. 5. Harbour View Village

Massing from Water View

6. If interest in the Harbour View Village project is not sufficient from related landowners or from the private sector development community, the City and Port would move to a 'backup' plan when funding allows. Elements of the backup plan include:

- On the Harbour View city property, retention of the majority of existing trees, combined with a developer proposal call for residential development to the east and west of the existing trees.
- On the Port Water Street Wharf property, the replacement of the Wharf with 'finger piers' to accommodate a recreational boat marine lift, tour vessels, potential seaplanes, fish off-loading by large and small craft, public display of boats (tall ships) and related marine wharfage activities.

Rationale:

- There is a need to balance public interest in creating dedicated park space on the prior Harbour View property with the potential for both economic development and job creation that this valuable property represents. As well, the City and Port need to find financially feasible ways to provide the public space and amenities in this area, and the private sector could be a key partner in that.
- The City has asked for a detailed assessment of the impact on fir trees of this proposal. It appears that up to ten trees would be removed, but the majority of the fir stand would remain and would be sustainable.
- The Implementation Steps section of this report provides more direction regarding key steps, and provides a discussion of financial concepts that might surround this project.

Industrial Re-investment at the Plywood Site

Recommendations:

7. The City has recently implemented a 'Bylaw to Provide a Revitalization Tax Exemption for the Heavy and Medium Industry Areas'. It is recommended that the opportunities for industrial reinvestment presented by this bylaw, the resolution of the softwood lumber dispute, and other recent changes in the Coastal Forest Industry be provided a period of five-ten years to be realized before making land use decisions that would forestall industrial opportunities.
8. In the land area immediately adjacent to the existing Port Authority berth and assembly yard, choices of new road alignments or other land use changes should consider carefully the land needs of the Port, with an eye to allowing additional space for Port use if that is feasible and economic. The potential for efficient transportation linkages by industrial road and rail should be maintained to the assembly wharf(s).
9. Specifically, the City and Port should encourage industrial redevelopment of the former 'Plywood' site to uses that are compatible and complementary with the APD sawmill and other adjacent land uses.
10. The City and Port, with related landowners, should co-ordinate and market a comprehensive industrial development including the City plywood property, the adjacent Crown foreshore, adjacent water lots, and surplus lands from adjacent property owners. Actual development of the property would be by the private sector.
11. The comprehensive development by the private sector should include:
 - Industrial development that takes full advantage of the unique marine exposure of the property, and the proximity of the Port Terminal and forest and marine industries of Port Alberni.
 - Provision for public access to the waterfront in a location that is compatible with the industrial development. Public access shall include a public right of way from existing City streets to the water's edge, as well as development of an accessible vehicular and/or pedestrian route to

gain access to the Alberni Inlet. Provisions shall include, at a minimum, the ability for the public to reach the water's edge, gain views of the harbour and its activity, and to be safe from industrial hazards.

- The industrial development should also provide a right of way to accommodate existing and future deep-sea cables that cross the property.
- Visual buffering of new industrial development from existing residences on Plywood Drive should be provided.

Refer to the Implementation Steps section of this report for a discussion of how to assemble and actively market this site.



Fig. 6. Industrial Re-investment Opportunities in the Plywood Site

*Rationale:***A Strategic Economic Asset**

The Port of Port Alberni's deep-sea facilities are a strategic asset for Port Alberni as well as for Vancouver Island as a whole. In spite of British Columbia's 27,000 kilometre coastline, the PAPA wharves are an unusual asset. There are only three places on Vancouver Island where cargoes can be handled for deep sea vessels; these are:

- Port Alberni Harbour Authority facilities;
- Nanaimo Harbour Authority facilities at Nanaimo Assembly Wharf and at Duke Point.
- Wescan Terminal at Cowichan Bay.

While the Ogden Point facility in Victoria can receive deep sea vessels, there is virtually no opportunity for significant volumes of cargo to pass through Victoria to reach it.

For the west coast of Vancouver Island and for direct access to the Pacific, there is no alternative to the PAPA facility with its road and rail connections that allow cargoes to move in and out. From Port Alberni, the Alberni Canal provides protected linkage to one of the world's busiest concentrations of marine traffic through Juan de Fuca Strait.

Marine Cargo Trade Impacts

The following multipliers are estimated for each port job that is created. The multipliers mean that each person year of port work multiplies 2.25 times to create an additional 1.25 jobs elsewhere in B.C.

Table 3: Multipliers for Port of Vancouver Maritime Cargo in B.C.

	Person Years	Wages	GDP	Output
Direct	1.00	\$58,836	\$71,999	\$158,983
Indirect	0.68	\$28,684	\$41,367	\$89,008
Induced	0.57	\$24,176	\$40,270	\$86,255
Total	2.25	\$111,696	\$153,636	\$334,246

Job Creation associated with Break Bulk and General Cargoes Handled in Port Alberni

Job creation in relation to tonnage, based on Vancouver data, is one person year of employment created for each 530 tonnes of lumber or pulp per year that is shipped.

Ship Building and Repair

Based on Vancouver data, for every \$130,000 of output from the ship building and repair industry, one person year of employment is created.

Capital Investment in Port Infrastructure

Based on Vancouver data, for every \$125,000 in capital investment in port infrastructure, one person year of employment is created.

Space Requirements

Cargo assembly consumes large spaces that must be close to deep sea berths. While break bulk and general cargo densities vary, as a rule of thumb, it typically takes about one hectare to store 5,000 tonnes of general cargo or project cargo. In addition there is a need for spaces for ship to shore cargo handling, and rail to ground transfer, and truck to ground transfer. Between the cargo storage area and the ship there has to be a 30 metre wide equipment operations apron. Hence, for two 200 metre long ships plus a 15 m spacing ship to ship, the minimum cargo operations area is about, 30 metres by 415 metres. Altogether, according to guidelines of UNCTAD (United Nations Conference on Trade and Development) the minimum functional area requirement for a two berth break bulk and general cargo facility is about 10 hectares. The Port Alberni Port Authority directly controls about 7 hectares. In short, the Port Authority does not have surplus space.

Required Transport Linkages

To operate effectively and to avoid being isolated from the sources and destinations of the cargoes it serves, the Port Alberni Harbour Authority requires linking transport services. Essential transport linkages include:

Connection to Coastwise Marine Transport – In order to link with the B.C. coastal barge transport system, an efficient means of barge to ship, ship to barge and barge to shore transfer is required. For barge to shore transfers, a barge ramp is the most efficient method. Hence, space planning for such a facility is required. Because barges have a smaller carrying capacity than deep sea ships, sea room is required in the harbour to moor additional barges. To make a port attractive to a shipper, say, a forest industrial exporter, the option to connect by barge to mainland container terminals is an important complement to the deep sea shipping capacity of the Port Alberni Port Authority.

Connection to Land Transport by Road and Rail – The port's industrial access road and its rail lines and right of ways require protection from inappropriate development. While the need for continuous of truck routes through the city of Port Alberni to the port area is obvious, the requirements for rail transport probably require comment. Rail transport is an important alternative to truck transport that is capable of carrying cargoes heavier and larger than can be moved on roads. For the rail access to the port area to be effective, the port's rail system requires protected right of ways for:

- The main line rail track,
- Say, three rail sidings to marshal and store rail cars. Since each rail car is about 17.4 metres long, to place, say, 40 rail cars, it takes a total about 700 metres of rail siding length plus about 100 to 150 metres of clear track at the switches to provide storage. For example, the total length and width required for a main line track, three sidings for 40 rail cars, and the switches is about 300 metres by 21 metres.
- A run around track to allow locomotives to turn around after it arrives is needed at the port's cargo area.

Potential Industrial Uses

The following types of industrial uses can be foreseen as potential for the Plywood site at this time:

Offshore Vessel Support Base

Port Alberni may be suitable as a smaller scale offshore support base similar to Port aux Basques, Newfoundland. Vessel repair services now in Port Alberni would complement such a base. The facilities typically required could include:

- a relatively tall industrial building equipped with an overhead crane
- sub sea testing facility (46m x 24m, 16m hook height)
- Heavy and small CNC (computed aided) machining capabilities
- CNC (computed aided) cutting & plasma burning (up to 12 inch plate)
- Exotic welding facilities



Figure 7: Offshore Vessel Support Base

Wood Products Manufacturing Facility

Traditionally these have included: softwood lumber, structural panels (OSB and plywood) and medium density fibreboard. More recently, forest research has raised hopes that off shore markets in Japan for hemlock to be regained by a new grade of hemlock kiln dried lumber (E120-F330). Since hemlock accounts for 60% of the coastal forest fibre supply, there are hopes for a revival of the declining coast forest sector. If markets for hemlock can be found, other species may be processed on an economic basis as well. Such changes may encourage re-investment in an industry that has been living off its cash flows without the ability or the incentive to re-invest. The size of the “plywood site” may lend itself as a location for a specialty sawmill that can extract higher values from an environment of larger flows of wood fibre in and out of Port Alberni.

The following diagram from the Wood Products Industry Advisory Committee report of March 31, 2006 illustrates the balance of factors necessary to sustain the forest sector in Port Alberni and elsewhere on the coast. The City of Port Alberni and the Port Alberni Port Authority, with their control of port industrial lands can have a positive impact on the costs of transportation, security of tenure of industrial lands, supportive zoning, and by securing land and marine access corridors.

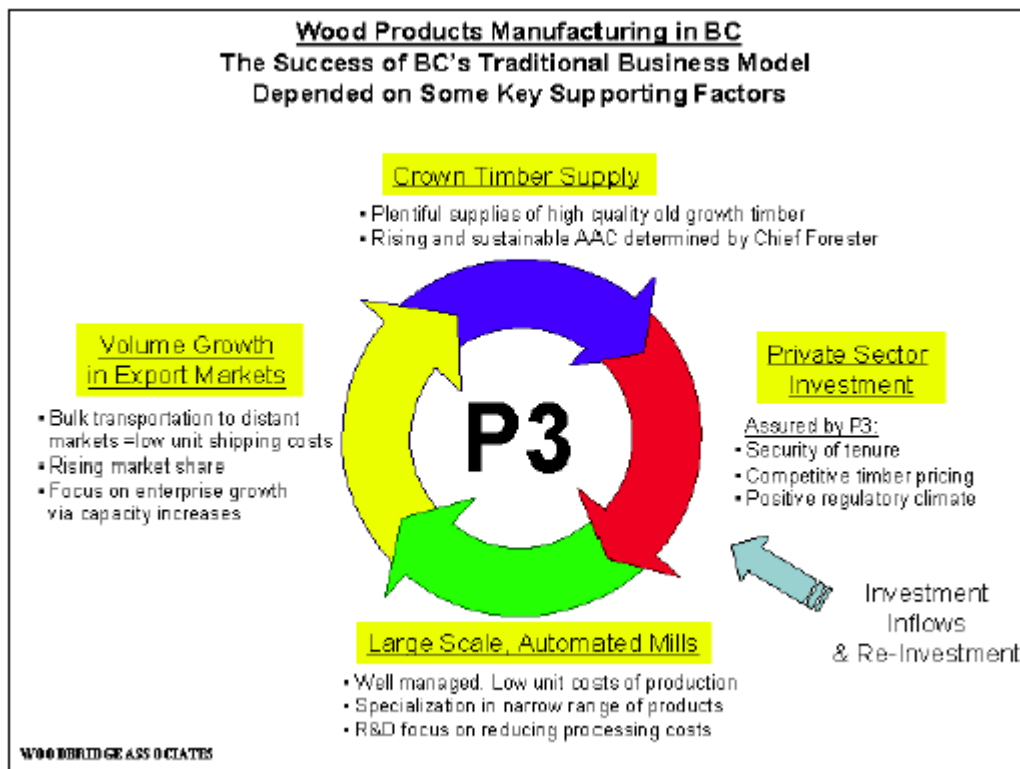


Figure 8: Wood Products Manufacturing in BC

Option 2 – Shipyard

Shipyard Layout Based on a Vessel Lift

By making use of the long but shallow site, the vessel lift arrangement for the proposed shipyard allows 6 major vessels to be serviced at one time. The types of vessels that would be serviced would include: small ferries, fishing vessels, tug boats, off shore support vessels, large pleasure craft and barges.

The lift shown in the drawing is 60 metres long and 18 metres wide. Similar lifts would be capable of handling vessels with a displacement of more than 3000 tonnes. This type of lift has no fundamental size limit. Yards with this type of lift handle the largest deep sea vessels. At this site, the main limitation

with respect to vessel size limits would have to do with the size of the site. Staying within the property lines and taking into account the business need to have the capacity to service a number of vessels at one time, probably limits the proposed shipyard to 60 metre vessels for the most part.

Some of the second row of vessels could be within buildings. These buildings would contain sand blasting and painting operations. Enclosed sand blasting and painting is environmentally friendly and makes it possible to apply high quality long lasting coatings.

The yard could be designed to handle vessels twice as long, if required. Doubling the design vessel length would be at the expense of being able to handle fewer vessels at one time. Further studies are needed to determine the optimum maximum vessel design size.

The photograph below shows a small yard with a 60 metre by 18 metre vessel lift.



Figure 9: Vessel Lift

The illustration below shows a mega pleasure craft being serviced the combination of a vessel lift and a dual walking beam system that allows forward and side to side movement of vessels within the shipyard.



Figure 10: Dual Walking Beam System

The photograph below shows a typical offshore support vessel being raised by a vessel lift. In the long term, Port Alberni may have the potential to a base for various types of offshore support vessels.



Figure 11: Vessel Lift for Offshore Support

Shipyards Plan Based a Conventional Marine Railway and Turntable

The illustration below shows the planned final arrangements for the Port Hope Shipyards in Victoria. The combination of the marine railway and the turntable allows a number of vessels to be serviced at the same time. Some of the radial tracks from the turntable lead to buildings for sand blasting and painting.

This type of shipyard plan may be feasible at the plywood site, although the lack of depth in the site prevents taking full advantage of the potential of the turntable. Some of the potential radial tracks could not be used at the plywood site because of interference from the close-in embankment.



Figure 12: Shipyard with Marine Railway

Recommendations (cont'd)

12. If, after a suitable period e.g. 5-10 years, prospects for appropriate heavy industrial development are not acceptable, the City and Port may consider a 'backup' plan that includes:

- Splitting the plywood site into three uses – a 'transition' industrial/commercial use at the north side, a housing/resort use at the south side, and integrated public waterfront access and boat launch site.
- The industrial commercial use should be configured to provide a noise attenuation barrier between existing and new noise sources and the proposed resort development. The noise attenuation may include a combination of building siting, berming and noise wall. Buildings may need to extend into the harbour to provide effective noise attenuation.
- Industrial commercial uses might include shipyard for both pleasure and commercial boats, as well as related boat storage building, machine shops, boat sales and related businesses. Ideally the shipyard uses should complement the proposed adjacent residential / resort housing.
- The residential portion could take the form of stacked townhomes, with about 200 units depending on the unit size.
- At seaside, the lower floor(s) would be restricted to storage and parking, to reduce the risk of tsunami loss of life. The height of the seaside units should be restricted by the need for noise attenuation from the existing sawmill (likely restricting the residential portion to two storeys above parking).
- On the upper level, near Plywood Drive, the townhomes site could be extended in co-operation with WFP to include the entire upper terrace. Noise attenuation would be provided by a berm along the east side of the development site, combined with a lowering of the grade of the site by about 3m. This would result in a development site that has views of the harbour, but that is below Plywood Drive. To avoid noise impacts, the development would be restricted to 2-3 storeys in height.



Fig. 13. Backup Plan: Mixed Use Reinvestment in the Plywood Site

- Units in this development should be required to have central air conditioning. This avoids the need to have open windows during hot summer nights, and thus reduces potential noise conflict with adjacent industry, which may have active night shifts.
- All residential units should have a covenant on title that restricts the right of owners to act against adjacent industrial users as long as noise decibel levels are within stated limits. The covenant would recognize that industry may operate 24/7.
- It could be an option that some residential units might be resort use, either by fractional ownership or 'lock-off' plans that allow owners

to market rooms to short term guests. The City would need to carefully consider whether such units are taxed as residential or commercial hotel.

- The public portion of the site could include road access, a public boat launch and associated vehicle and trailer parking. The existing pier would be repaired to support safe public access, and may include a limited amount of commercial development with housing/resort units over. Small floats for convenience tie up would be associated with the pier and public boat launch. A waterfront boardwalk would front the residential/resort development. Architectural controls and landscape area would soften the interface with the adjacent commercial /industrial building, and add to noise attenuation at the 'bend' in the access road.
- The site is neighbours with the Tseshah First Nation to the south. Consultation with the Tseshah should be a part of detailed planning if this option is chosen. It is possible that the Tseshah plans for their I.R. might be complementary, and /or that a partnership might evolve.

Rationale:

- If industrial re-investment in this valuable piece of waterfront property is not forthcoming over a reasonable period, or if at the end of that period there appears to be a very low chance or such reinvestment, it may be necessary to look at alternate uses that will meet public objectives and be economically viable.
- Owners of the APD sawmill (now Western Forest Products) hold a covenant, which prohibits the use of the Plywood lands for overnight accommodation. This is to protect the industrial use at the APD from complaints about noise. This covenant would need to be lifted or replaced with alternate wording for the type of mixed residential commercial development envisioned to proceed.
- If this option were to proceed, both WFP and the adjacent Tseshah First Nation may have an interest in bringing in some of their lands and collaborating as partners on the project.

- To provide a buffer between the residential and existing industrial uses, there should be an effective noise barrier. This could be in the form of landscape berms or noise walls, but also could include commercial building masses that are not as sensitive to overnight noise disturbance. A Boat Storage and Small Craft Service Centre is one option that may, in time, be viable and compatible as such a buffering use. Precedents for such a use are described below.

Boat Storage Building and Small Craft Service Centre

Description - A high capacity rack storage system may be possible to provide dry land storage for boats up to 40 feet long. Rack storage offers a greater density of boat accommodation at a lower cost than in slips. Some marinas charge more for rack storage than for wet moorage because the market favours this type of accommodation. For the mariner, rack storage provides protected storage for boats, which reduces maintenance costs. There is also an environmental argument for rack storage. Boats in rack storage may not require environmentally damaging anti-fouling treatments. Discharge from boats while in storage can be better controlled.

Business Model - The business model for this type of facility would include: boat storage services, boat maintenance services, boat building and repair services for clients and others, and boat concierge services. By co-locating boat repair and boat storage, the costliest capital investments are spread over a wider revenue stream.

User Benefits - A rack storage and mechanized handling facility may be particularly attractive to seniors or to persons who may have difficulty managing their boats at a boat launch. The covered land storage offers the boat owner reduced maintenance costs, longer boat life and easy launching of their vessels. Furthermore, the boat owner need not bear the expense of a boat trailer and a large vehicle to tow the trailer. Overall, the rack storage building substantially reduces the cost of boat ownership. There are some environmental benefits to this type of boat storage because boats on land in covered storage probably do not need to use anti-fouling paints and generally will require much less painting and scraping.



Figure 14: Boat Storage Building Interior



Figure 15: Boat Storage Building Exterior

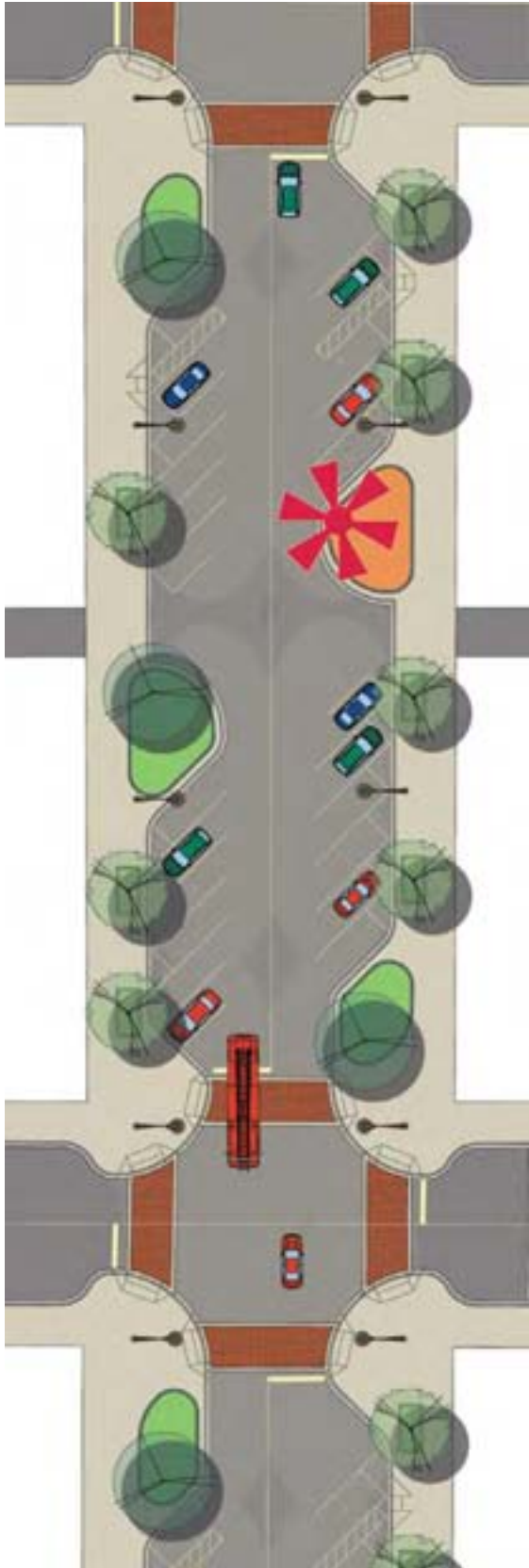


Fig. 16 - Argyle Streetscape Detail Plan / Perspective

Streetscape Improvements

Recommendations:

13. The City should proceed with phased improvements to both Argyle Street and 3rd Ave.

14. Improvements on Argyle Street should be designed to make a more strong link for motorists and pedestrians between Harbour Quay and 3rd Ave. To achieve this objective, improvements to Argyle should include:

- Reducing the travel lanes from two in each direction to one in each direction, without a central median.
- Maintaining angle parking on both sides of the street.
- Creating new, wider sidewalk areas on both sides of the street.
- Sidewalk areas should include space for through-pedestrians near storefronts, with the remaining space between the through-walk and the parking treated as flexible space for outdoor cafes, interpretive displays, plantings and street furniture.
- Special features along the flex zone should be delineated by raised planters and/or heritage artifacts and interpretive displays. Spacing of these planters and plantings will be provided with regard for views to building facades and accessibility from parking to sidewalk.
- A key area in each block should be set aside for a sidewalk 'attraction'. The spacing of these attractions should be designed to 'pull' pedestrians from one block to the next. The



attractions should include elements that will encourage user participation and photo opportunities. Themes may include maritime heritage, industrial heritage, first nations heritage, art and culture.

- Sidewalk budes should be designed to minimize the crossing distance at crosswalks, while at the same time respecting the turning radius of large logging trucks, buses and other vehicles where required. Visual barriers should be restricted in the sight distance triangle at each intersection. Street entertainers or buskers should be organized for one corner per intersection, with committee approval for quality control.
- Overhead utilities should be under-grounded at the time of the sidewalk replacement. Location of utility boxes should be carefully coordinated with paving and other streetscape elements.
- New street lighting with decorative poles should be completed at the time of the streetscape improvements. Pole and Fixture should reflect the maritime heritage of the site.
- Trees and plantings should be supported by an automatic irrigation system to reduce maintenance requirements.

Argyle Street Furnishings:

Sidewalk Paving

Type: Broom finished concrete.
Pattern: 1m x 1m square score /expansion joints.

Street Lighting

Type: 6.7m(22') fixture height, 30m spacing.
Style: Marine Heritage Luminaire.
Colour: Bronze Aluminum.
Accessories: Banner Arms, Hanging Basket System, Electrical Outlet (dual).

Street Tree Planters

Type: 5m(L) x 2m(W) 1.2 – 2m(D) / 20cu.m.
Treatment: Local stone planter wall and cap.

Trees

Form: Evergreen in curb bulge planters.
Upright /columnar deciduous trees in sidewalk planters.
Spacing: 15m. on center. Irregular Pattern.

Bench

Type: Heritage style to match light fixture character. Metal frame.
Colour: Bronze Aluminum.

Trash Receptacles

Type: Heritage style to match bench character. Metal frame. Side access door.
Colour: Bronze Aluminum.

Bicycle Rack

Type: Locking frame and fork style.
Colour: Bronze Aluminum.

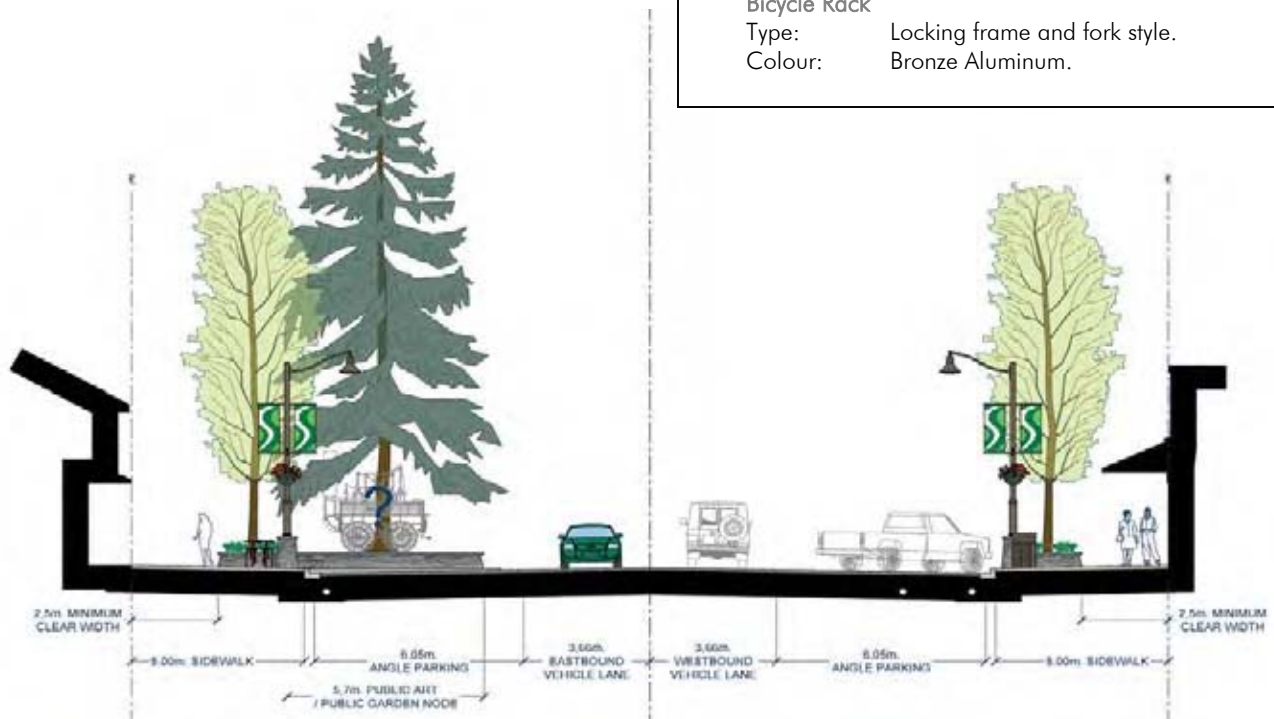


Fig. 17 - Argyle Streetscape Typical Detail Section

- Marked and signalized pedestrian crossings should be installed at the intersection of Argyle Street and Harbour Road, at the same time that curb and sidewalk adjustments are made to minimize the crossing distance while allowing truck movements.

Rationale:

- The four-lane width of Argyle Street is more than adequate for the traffic loads that are using the street now, and that are likely in the future.
- Greater public benefit can be gained by using a greater proportion of the street right of way for pedestrian and landscape uses.
- This change in the street use will promote more walking up and down the street, and will make adjacent properties (both private and City-owned) more attractive for retail and residential occupancies.
- The resulting combination of street character and reinvestment in buildings will provide the better link between Harbour Quay waterfront and Uptown Port Alberni.
- This highly visible street redevelopment will also stimulate renewed confidence for private business and property owners in the area.

15. 3rd Ave streetscape improvements should be designed to enhance and update the existing streetscape, and to make it more pedestrian friendly while discouraging loitering:

- New curb and planting should be installed to existing painted median islands on the 3rd Avenue hill.
- Pedestrian crosswalks should be marked on all sides of intersections crossing 3rd Ave in the study area.
- New street lighting fixtures should be installed to replace the existing outdated fixtures.
- All existing planters should be retained. However, they should be adjusted by removing the existing blue wood cap and replacing this with a combination stone cap and clusters of individual-scale seating. Planting in the planters should be replaced with new materials with emphasis on flowering and arching habit over the stone cap.
- A hanging basket and banner program should be continued in the Uptown Area, with either automatic irrigation to baskets or those with built-in watering reservoirs. Hanging basket locations between planters is desirable.
- Minor repairs to existing sidewalk and crosswalk paving should be completed. Such repairs should match the main paving pattern as closely as possible.
- An aggressive and on-going cleanup program to remove all litter and graffiti should be maintained – to say 'We care about our Uptown!'

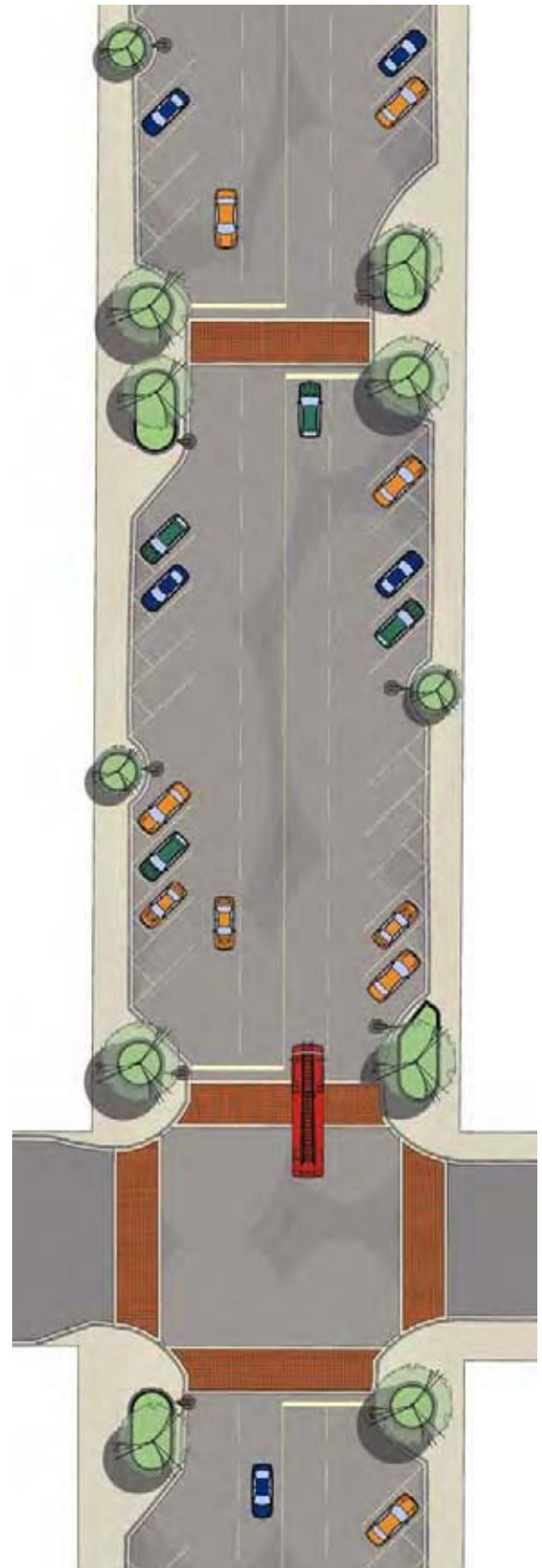


Fig. 18 - 3rd Ave. Streetscape Detail Plan

Rationale:

- Better treatment of existing planters will improve appearance, reduce loitering, and reduce maintenance costs.
- New tree plantings along 3rd Ave. can be selected to provide a high canopy over the street, but to remain above commercial displays and signage.
- New street lights will both increase street comfort at night, and update the character of the area to current aesthetics.
- An update to the deployment of banners and hanging flower baskets may provide more colour in-between planters. Current systems tend to be at the planters, and their effect is somewhat lost in the existing trees.
- At the same time, the improvements should recognize the high quality of the existing stonework and existing sidewalk paving. The character that these past investments has created should be enhanced rather than replaced by the improvements.

16. A new system of Wayfinding Signage should be implemented in the area between Harbour Quay, Argyle Street and 3rd Ave.

- Signage at key locations should be pedestrian scale, and should include maps that clearly show attractions, routes between them, and 'you are here'.
- The Wayfinding Signage should be integrated into the street lighting and site furniture system.

Rationale:

- Signage is an important symbol of how much residents and businesses care about their community. Carefully designed, small and artful signage is a key to being attractive to both residents and tourists.
- Visiting tourists are not likely to be aware of the relationship between Harbour Quay and the Uptown Village. Wayfinding signage is a relatively low-cost means of creating a linkage between the two areas.
- Signage that is integrated with other street furniture creates less clutter. Less clutter provides a better setting for retail businesses to use their window displays to attract customers.

3rd Street Furnishings:

Sidewalk Paving

Type: Existing concrete and brick inlay

Street Lighting - Renovation

Type: 6.7m(22') fixture height, approx. 30m spacing. On existing pedestals.

Style: Heritage Luminaire.

Colour: Bronze Aluminum.

Street Planters and Benches

Type: Existing stone planters, wood seat cap.

Treatment: Replace wood seat ring with stone cap and 1 section of seating.

Bench: 1.2m(4') Surface mount. Heritage style to match light fixture character. Metal frame with wood seat slats.

Colour: Bronze Aluminum.

Trees

Type: Existing. Replace as needed with columnar or high-branching canopy species.

Trash Receptacles

Type: Heritage style to match bench character. Metal frame. Side access door.

Colour: Bronze Aluminum.

Bicycle Rack

Type: Locking frame and fork style.

Colour: Bronze Aluminum.

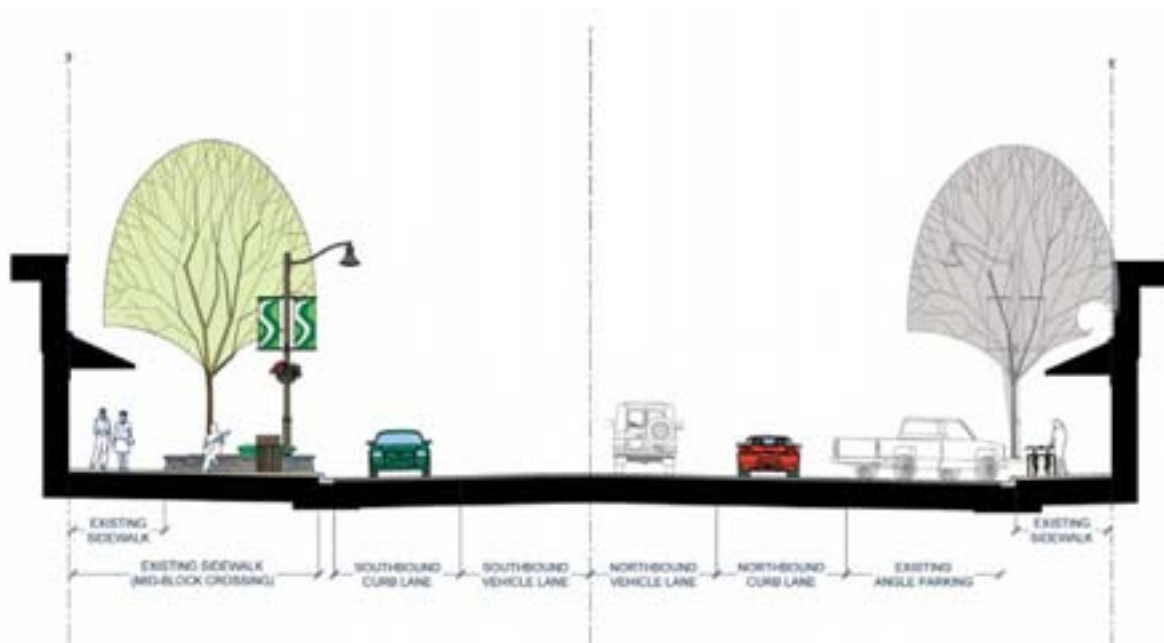


Fig. 19 – 3rd Ave. Streetscape Detail Section

Innovative stormwater engineering could be included along Argyle Street as a part of roadway improvements.

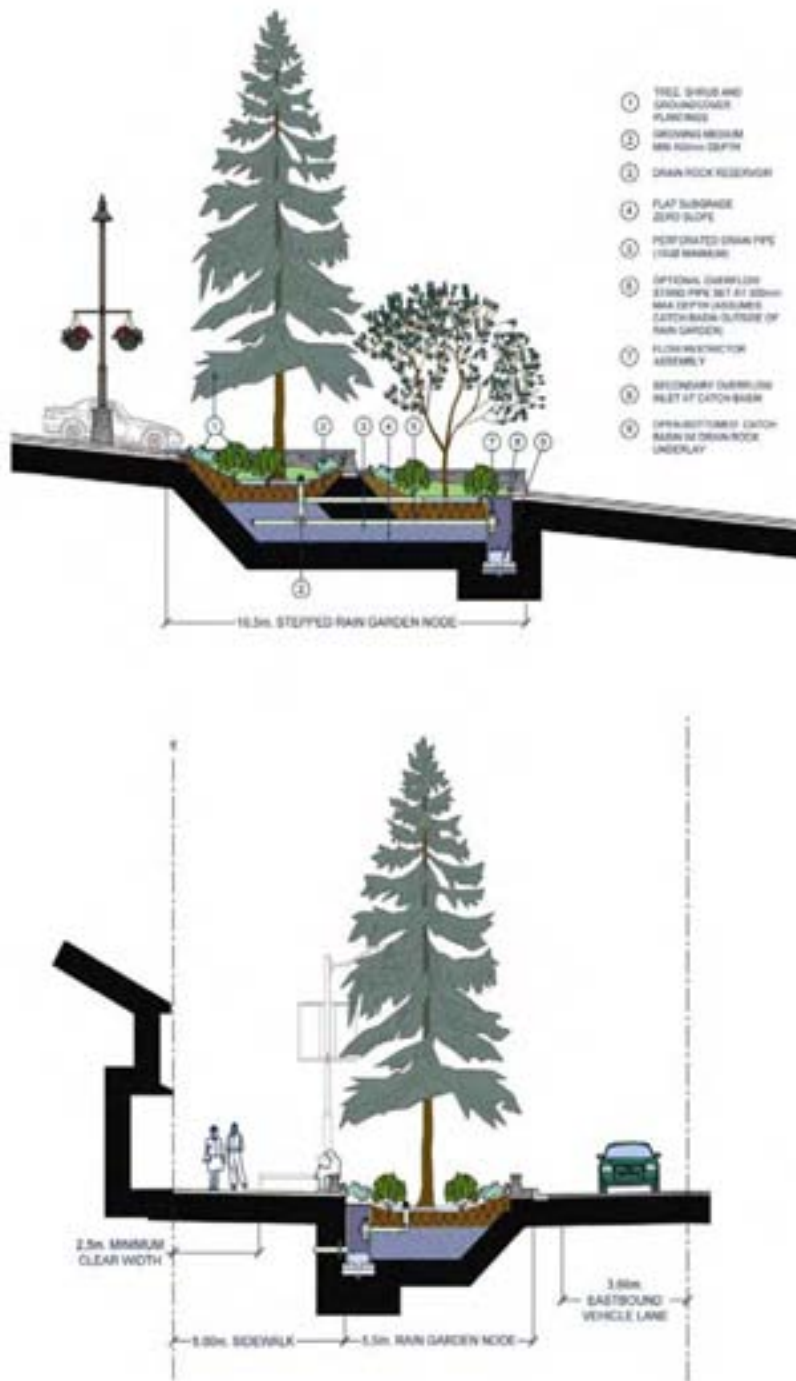


Fig. 20 - Argyle Street Rain Garden Detail Section and Profile

- Given the width and area of proposed curb bulges and the resulting 'nodes' there is an opportunity to implement rain gardens along Argyle, if this could be funded by senior agencies as a demonstration of stormwater treatment on a steep, urban retail street.
- Rain gardens are designed to capture run-off from adjacent impervious surfaces. The garden itself acts as a sponge and filter for stormwater. It improves downstream water quality and allows infiltration to reduce the volume of stormwater.
- The rain garden would be normally dry and would serve as an ideal demonstration / interpretive site for the display of native plants and plant ecosystems that are of key importance to many local industries.
- Figure 6.6 is intended to show one of many possible rain garden configurations that would be possible along the roadway. The final design of any rain garden configuration will need to be engineered to suit the specifics of the site's soil and climatic conditions.

Rationale:

- Senior government agencies are promoting the use of 'green infrastructure' to treat stormwater.
- Port Alberni could be an excellent demonstration project if outside funds could be attracted.



Fig. 21. Argyle / 3rd Streetscape Overview

Uptown Village

Recommendations:

17. The City should co-operate with landowners in the block between 3rd / 4th Avenue and Argyle / Angus Streets to plan and market a comprehensive redevelopment as the core of the Uptown Village.
18. Land Use Mix in this block should incorporate a significant residential component, to complement retail uses on the 3rd Ave and Argyle St. frontages. To take advantage of the mountain and ocean views from the site, and for project economics, the City should consider mid-rise to high rise residential point towers if there is market interest.
19. The existing theatre would remain in this block. It is also important that either existing or new retail storefront be provided along Argyle and 3rd St on this block, so that there is continuity in the retail streetwall. New storefront development could be in the form of 'lofts' or 'work-live' units – these combine living quarters with retail or artisan workshop space, usually two or three storeys. This retail development may be punctuated by entrances and related landscape to the residential component, which may be located over parking garage in the central areas of the block.
20. The frontage of the block along Argyle and Angus may mix urban frontage with a more residential, green appearance. The use of streetscape plantings, landscaped terraces, courtyards and carefully design signage and entrances, as well as landscaped lid over parking garage would provide a 'prestigious' setting for the residential.



Figure 21a and 21b: Uptown Village Parking and Roof Level Plans

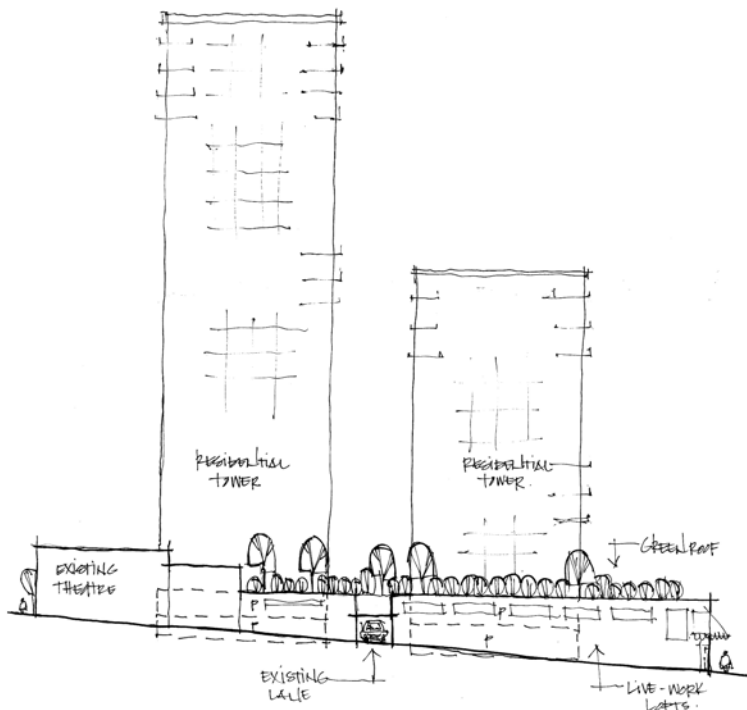


Fig. 22. Uptown Village Redevelopment Potential

Rationale:

- A key component of revitalization of downtown business districts has been the re-introduction of residential within walking distance of the retail component.
- A comfortable walking distance is 5 minutes, which typically represents, in the literature, a 400m distance. Distances greater than this tend to lead to people considering use of their cars.
- The residential condo component of the project, if it generates a suitable number of units, may in effect subsidize the creation of the retail/loft space. For this reason we suggest that the site be packaged as a comprehensive development to one developer for both the residential and commercial components.

Fishermen's Harbour Improvements

Recommendations:

21. The Port should work with Western Forest Products (WFP) and Small Craft Harbours (DFO) to improve access to the existing Fishermen's Harbour area. Improvements should include:

- Co-operating with WFP to make available a strip of land approximately 20m wide above the top of bank on the north side of the marina.
- Improving this land to provide two lane vehicle access and parking close to a new dockhead at the western end of the parking. This new dockhead and parking would provide major improvements in convenience to access the floats at a more central location. To avoid this new parking becoming plugged with long-term vehicles, the new parking should be 'pay-parking'. Revenue from this may provide funds for land tenure, security or related improvements.
- Relocate the Wharfinger's residence from its current location south of Port Fish to a new location near the new dockhead. This move will reduce congestion at Port Fish and also provide better security for the new dockhead and for Fishermen's Wharf.
- Provide a public walkway at the top of bank between the new parking and the marina. Extend public access onto the floats during daylight hours, so that the public can explore this section of the harbour. The Wharfinger would close the gate to the public, with key-only access in off-hours.
- Create a fence and landscape buffer between the new parking/walkway area and the Somass Mill yard.
- Ensure that site plans allow for a 'T' turnaround for semi-trailer trucks at the Port Fish dockhead.
- On the Somass Mill side, ensure that the land tenure arrangement retains adequate access to their mechanical shop and related truck circulation routes.



Fig. 23. Fishermen's Harbour Improvements

Rationale:

- The existing Port Fish and adjacent Fishermen's Harbour dockhead are highly congested. There is insufficient space for truck movements, leading to trucks sometimes backing into the site from Harbour Road.
- The length of walk from parking area (e.g. east of Harbour Road) to boat along the dockhead and floats is too far. This is especially true for loading purposes. There is a need for short term convenience parking closer to the boats.
- The Shoreline Master Plan states the objective of public access improvements along the south side of Somass Mill lands. Part of the intent of this objective is to improve public views to the working harbour. In this proposal, the public access would be partly on land, and partly by allowing daytime public access to existing floats.
- A proposed lookout added above the floats could provide views of boat activity and the larger harbour, as well as being an identification landmark for Fishermen's Harbour.

22. If the existing Water Street Wharf is not replaced, the Port should ensure that suitable wharfage is retained in other locations for medium-sized fisheries and recreational vessels. To do this:

- The Harbour Quay Wharf at the foot of Argyle should be retained for wharfage uses. New buildings should not be constructed that would restrict boat or truck access to the Wharf. Existing parking on the Wharf should be monitored and removed if it is, at some point in the future, unreasonably impeding use of the wharf by visiting vessels.
- The Port and SCH should investigate the potential for float extensions for larger craft at the northwest corner of Fisherman's Harbour – just outside the Somass Sawmill site.
- Provisions for small craft offloading of fish at the Fishermen's Harbour site should be ensured.

Rationale:

- Space for tie-up of large craft in the Port is limited. There should be an approach of no-net-loss of such space, until such time as it is proven that there is not demand for the tie up wharfage.
- If the Water Street Wharf is not replaced, the proposed float extension would provide tie-up for the displaced larger craft.

Harbour Quay Detailed Planning and Gradual Improvements

23. . The City and Port should facilitate a separate, more detailed planning process for Harbour Quay gradual improvements. Steps in facilitation might include:

- The City and Port co-operating with Pacific Tugs, Alberni Engineering and adjacent landowners to plan a comprehensive redevelopment on the properties west of Bird Ave. to marine industrial /commercial uses.
- As a part of this redevelopment, the City should consider making all or part of Bird Ave. available for sale or land exchange.
- The Port should work with its existing tenants and consider whether there is a role for the Port as a landlord on all or part of these lands.
- The City may consider how existing buildings or future buildings in this area might accommodate (short-term or long-term) some of the City's tenants that might be displaced by changes in the Farmer's Market area or by needs for expansion. It is recommended that the Farmer's Market remain at its existing location at Harbour Quay.

24. Existing play area, outlook, and marine/tourist oriented uses at Harbour Quay should remain.

- The special marine quality of Harbour Quay, in particular at the western tip, is an important public and tourism asset to the Port Alberni region. We do not envision dramatic changes to land use in this area. New minor development or fire access should be designed to reinforce the maritime character of this site.

Rationale:

- Existing businesses in the Harbour Quay area are surviving and growing. To accommodate business growth, there may be a need for a gradual expansion of space and reorganization of business locations at the Quay. It is important for the City and Port to facilitate (but not subsidize) the growth of these businesses.
- Harbour Quay west is a successful landmark that should remain.

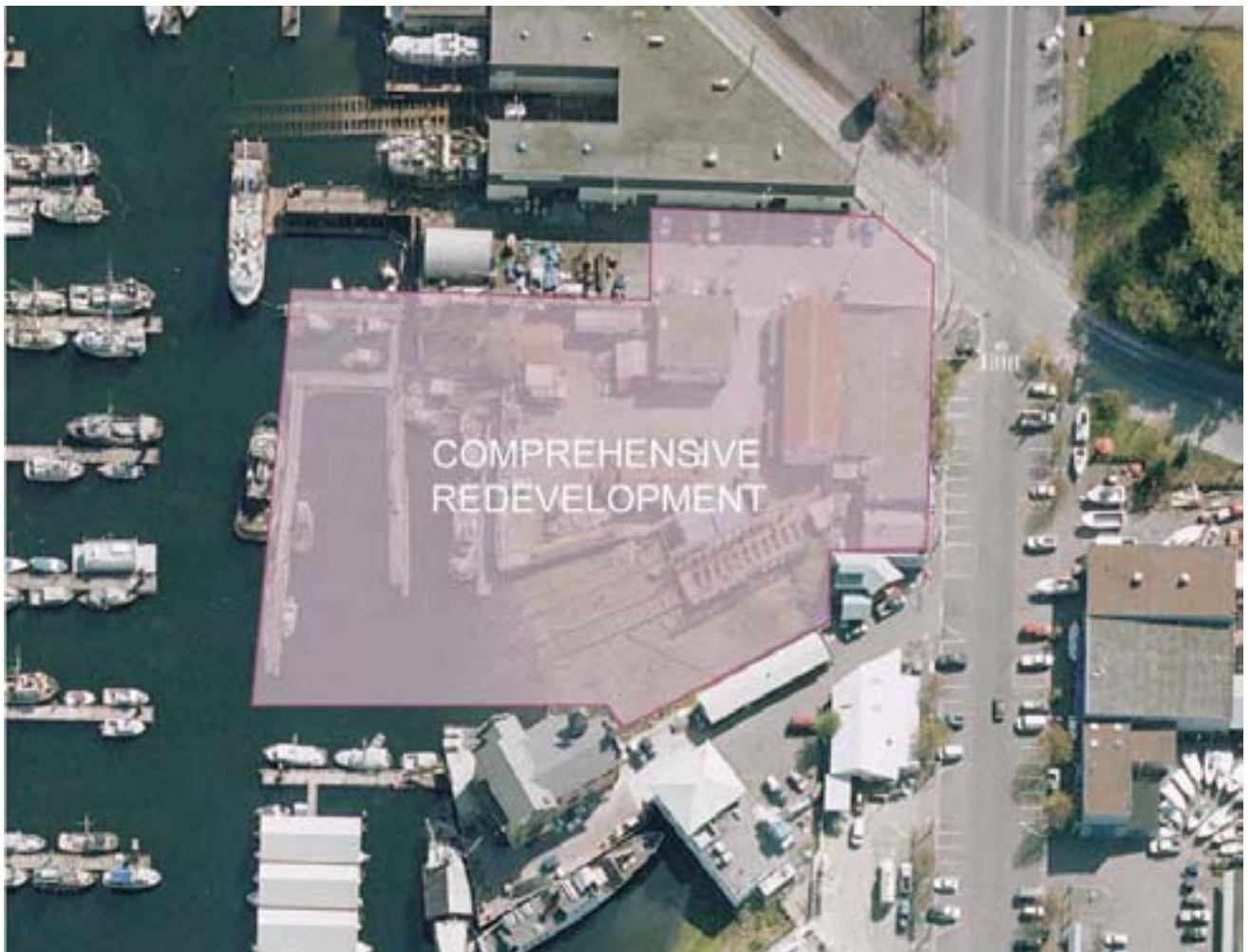


Fig. 24. Harbour Quay North Comprehensive Redevelopment Site

Cruise Ship Infrastructure

Recommendations:

25. The City and Port should continue to encourage the Cruise Ship Industry to treat Port Alberni as a stop-over. A key to attracting the cruise industry is to create the attractions and activities that will engage cruise visitors.
26. Initial Cruise Ship visits may be handled by existing infrastructure combined with temporary tents if required. When regular Cruise Ship visits are occurring, the City and Port should co-operate to create more permanent facilities that serve both Cruise Ship visits and other events, including:
 - Walkway, gates and landscape improvements to provide a welcoming and secure route from the Cruise Ship dock (the Berth #3 area) to the vicinity of the existing Harbour Quay Lighthouse. Gates in this area should be designed to provide secure landing for foreign tourists arriving from a Cruise Ship at the wharf to the south of the Lighthouse, or from cruise ship tenders dropping at the floats to the north. The gating arrangement may also facilitate the use of the 'Expo float' by helicopters or float planes.
 - A secure holding area near the existing Maritime Gallery. We envision a fenced and nicely landscaped courtyard and display space around the Maritime Gallery. This would function as a display space and garden for all functions of the Maritime Gallery, but would provide secure space for cruise ship passengers waiting to clear customs.
 - A customs clearing centre may be necessary if visits warrant. Initially, this might occur at the Maritime Gallery or an adjacent tent or portable building within a fenced courtyard. If traffic warrants, a permanent building could be created. However, this building should be designed for other public assembly uses and special events, in between Cruise Ship visits. There may be a small commercial component to the building as well as public washrooms.

- Beyond the customs clearance area, Cruise Ship guests will choose from a variety of excursions. Some will walk to Harbour Quay and up Argyle to 3rd, and high-quality experience for the length of this corridor is required. Others will walk to the steam train. Many others will enter buses or other vehicles to go to attractions like Cathedral Grove or Pacific Rim. A parking area to accommodate buses and taxis/vans is required in this area. This parking area will also accommodate these vehicles during all special events on the waterfront, and would be a pickup point for the shuttle transport when it is running.



Fig. 25. Cruise Ship Infrastructure

Rationale:

Table 3 provides an analysis of the strengths and weaknesses that face Port Alberni in attracting the Cruise trade.

Table 4

Port Alberni Cruise Trade Strengths and Weaknesses	
Item	Description
Strength – There is a safe, calm and reliable approach waterway.	Years of commercial deep sea traffic in the Alberni canal have put in place navigation aids, defined channels and years of pilotage experience.
Strength – Deep sea berths and a bus tour mobilization area is available.	Port Alberni Harbour Authority's deep sea berths and cargo assembly areas provide excellent facilities. Unless cruise traffic or break bulk traffic become intense, cruise ship calls and normal commercial break bulk ship calls can co-exist.
Weakness – Making a cruise port call at Port Alberni is a significant time commitment on a typical six or seven day cruise.	Initially, traveling up and down the Alberni Canal and choosing to go along the west coast of Vancouver Island may be logistically suitable only for positioning cruises at the beginning and end of the summer cruise season. For mid-season cruises, it will require a strong set of attractive land excursions to create a business case for a cruise to Port Alberni and along the west coast of Vancouver Island
Strength – A cruise including Port Alberni and other calls on the west coast of Vancouver Island is a fresh concept offering ample capacity and flexibility.	Conventional cruises based in Vancouver and Seattle, and going to Alaska are approaching saturation in some areas. For the cruise lines, scheduling flexibility is very limited and opportunities for revenue growth on traditional cruise routes are limited. Hence, Port Alberni and the west coast of the Island may have the potential to offer the growth that cruise lines seek.
Weakness – The financial incentive for cruise lines to call at Port Alberni does not yet exist.	It will be necessary to formulate business plans for excursions from Port Alberni from the harbour area. To attract the cruise lines, these excursions will have to offer revenue sharing between the cruise lines and the tour operators. Furthermore, excursions and tours and other ports of call will have to be established at other points on the west coast of Vancouver Island. It is in Port Alberni's interest to assist and facilitate the development of other port calls on the West Coast of Vancouver Island.
Strength – The potential to create cruise tour excursions exists.	Port Alberni, the Alberni Canal and other points on the west coast of Vancouver Island have much to offer a tourist. Road, rail and marine infrastructure exists and has spare capacity that could be used for cruise based excursions.

From the strengths and weaknesses of Port Alberni as a cruise ship port of call, it is apparent that one particular weakness is conspicuous. There is a need for cruise excursions based out of Port Alberni. If the City has an interest in pursuing the cruise ship trade, then it will have to be pro-active in creating feasible business models for excursion companies. Given the significant regulatory issues involved with creating such excursions using rail and road infrastructure and taking advantage of regional tourism attractions, newly forming tour companies are likely to need assistance from the Port/City.

Cruise Trade Impacts

While recognizing that the key to attracting Cruise trade is the partnering with Cruise Lines on tours and attractions, the economic stimulus of the Cruise Trade would be welcome:

- Based on a 2005 study of Vancouver cruise ship economic impact, each cruise passenger visiting from his ship on a day excursion spends an average of \$56 per day on his visit. A day trip results in an average of 5.7 hours spent on shore.
- Based on data from Victoria cruise traffic in 2006, each ship call will bring an average of about 2,200 passengers and 1,000 crew to the port.
- Over the years, the trend has been to a steady increase in cruise ship passenger capacity. Currently, the largest cruise ship is the Freedom of the Seas and it carries 3,600 passengers. Larger ships are being built; the Genesis series of cruise ships will carry up to 6,400 passengers. These large ships with their large passenger capacities have implications for economics benefits and for the cost of providing facilities and services on the basis of peak demand.

- In the Port of Vancouver, each overnight cruise visitor spends an average of \$301 per day. The breakdown of spending is as shown in the following table:

Table 5: Breakdown of Typical Overnight Cruise Visitor Spending in Vancouver

Category	% of Total Spend
Accommodation	42%
Food and Beverage	21%
Recreation	4%
Retail	27%
Transportation	6%
Total	100%

Source: BREA Study and 2003 Cruise Passenger Survey.

- In the City of Victoria, most cruise ship calls fall on a Thursday, Friday and Saturday. This timing is based on seven day cruises that start in Seattle, go to Alaska and then, on the return trip, call in at Victoria before returning to the Seattle home port. While the patterns for future ship calls to Port Alberni may be different, there is likely to be a certain peaking of arrival patterns which will impact the design of passenger receiving facilities and, incidental services that the City of Port Alberni may have to provide.

Shuttle Transportation

Recommendations:

27. The City and the Port should encourage the private sector to create a 'circle route' shuttle that provides a connection for tourists and residents between following destinations:

- Clutesi Haven Marina
- Victoria Quay
- Johnston Street Retail
- Uptown Port Alberni
- Harbour Quay and E&N Station
- Port Alberni Harbour

28. The Shuttle should provide both land and water transportation in a co-ordinated way. Approaches might include:

- Use of an amphibian vehicle, similar to Halifax Harbour, Ottawa River, or other points. This would require boat ramp access, or:
- Combination of a land based shuttle vehicle, with water-based boat vehicle tour. The schedule of the two vehicles would be co-ordinated so that they create a convenient circle tour. It also may be possible that there are several different marine tours available, ranging from First Nations Canoe to weather-protected pocket cruise.
- The water-based shuttle might originate from Harbour Quay and Victoria Quay, or may originate at points further south (Plywood Site) and further north (River) if landing were available. Tours might allow pickup and drop-off at any of these points.
- In both cases, all tours would be narrated to interpret points of interest, local heritage and culture, and marine/industrial operations.
- The land-based shuttle would allow pickup and drop-off at several points, so that users could walk to enjoy an area for a time and then rejoin the tour.

Rationale:

- A key term of reference for this study was to improve the linkage between the waterfront and the Uptown of Port Alberni.
- The Argyle Street hill is steep enough and long enough to be a challenge to many pedestrians. Many recommendations in this report will increase pedestrian usage, but there will be many pedestrians that will find that walking up and back down the Argyle Hill is too far.
- Therefore, to bring tourist and cruise trade into the Argyle St. and 3rd Ave. retail areas, there will be a need for some sort of shuttle transportation. However the need for the shuttle will be seasonal.
- To be economic, a shuttle probably needs a larger market than just the traffic running up and down Argyle Street.
- A shuttle that connects tourism and retail destinations across the City and waterfront has a better chance at viability.

7. Implementation Steps

Implementation Recommendations:

29. Once the report recommendations are considered and adopted by City Council and the Port of Port Alberni Board, early next steps would include:

- Arranging a proposal call for City-owned land along Argyle.
- Creating the partnership organization for marketing the Harbour View and Industrial Reinvestment projects.
- Organizing appropriate budgets to support these partnerships and marketing efforts.
- Proceeding with work to create marketing packages, and undertaking the marketing process.
- Co-operation with WFP and Small Craft Harbours to fund and implement the changes at Fishermen's Harbour.
- Fund-raising applications for improvements to Argyle and 3rd. Related contract drawings and details in preparation for construction to proceed in a phased manner.
- Land Use and Redevelopment Packages and marketing of redevelopment of the Uptown Village block.
- Land Use and Redevelopment Studies of the Harbour Quay properties west of Bird Ave.
- Continued marketing for Cruise Ship visits, and monitoring of needs for related multi-use infrastructure.
- Organization of the Shuttle.

The Implementation Steps section of this report includes concepts and strategy for starting these tasks.

Financial Approach in Concept

Implementation Recommendations:

30. Funding of the Uptown and Waterfront Redevelopment should be a shared responsibility, with funding sources to include:

- City of Port Alberni
- Port Alberni Port Authority
- Other Benefiting Landowners
- Land Sale and Lease Revenue
- Small Craft Harbours
- Private Sector Funding
- Volunteers, Grants and Donations

Rationale

A full partnership for funding is necessary to realize the objectives while minimizing the impact on the taxpayer and the Port, maximizing the economic development and job creation, gaining efficiencies in costs, and recognizing the substantial potential for the role of the private sector, community volunteers and outside funders.

Table 6 provides a conceptual framework for funding of the public component of the Implementation Strategy.

Table 6 is only an illustration in concept. The figures shown are not budgets, but placeholders to show an approach.

In Table 6, early actions create a cash flow, which is invested in marketing and feasibility studies. These lead to larger land sales or leases, which are used to fund capital improvements and later phases.

The point of Table 4 is to illustrate how the public sector cash flow can be, in the end, neutral. Table 6 also shows how decisions could be made in an orderly way to delay later, major projects, if early steps were not successful. This is the essence of an adaptive management strategy.

However, because this will be a co-operative effort among partners, and each partner has its own mandate and responsibilities, the actual funding arrangements will be more complex than what is shown in Table 6. Some funding and costs will be shared among the partners (e.g. Harbour View Village or the Plywood Site), whereas other items will be separate (e.g. City land sales and City streetscape improvements, or Fishermen's Harbour and Small Craft Harbour Improvements).

It is also possible that partners will invest some cash up-front in order to kick-start the process, and then retrieve this funding as projects proceed.

Table 6 assumes that the Argyle Street Improvements will warrant 50% grants from senior government funding programs. Fishermen's Harbour Improvements are not included –assumed funded by the Port and Small Craft Harbours.

Table 6 Conceptual Framework for Public Sector Plan Funding

(note: amounts are conceptual only and do not represent budgets)

Partnership Project	Conceptual Cash Flow	Cumulative Cash
Argyle Land Sale A	\$400,000	\$400,000
Argyle Land Sale B	\$400,000	\$800,000
Harbour View Village Feasibility / Marketing	-\$100,000	\$700,000
Plywood Site Feasibility / Marketing	-\$150,000	\$550,000
Argyle Streetscape Phase 1 (after 50% grants)	-\$500,000	\$50,000
Harbour View Village Lease Down Payment	\$120,000	\$170,000
Uptown Village Feasibility / Marketing	-\$100,000	\$70,000
Harbour View Village Lease Balance	\$1,080,000	\$1,150,000
Argyle Streetscape Phase 2 (after 50% grants)	-\$500,000	\$650,000
Plywood Land/Water Lease	\$1,000,000	\$1,650,000
Argyle Streetscape Phase 3 (after 50% grants)	-\$500,000	\$1,150,000
Argyle Streetscape Phase 4 (after 50% grants)	-\$500,000	\$650,000
3 rd Ave. Streetscape Improvements	-\$500,000	\$150,000
Uptown Village City Land Lease / Sale	\$50,000	\$200,000
Harbour Quay Comprehensive Development Study	-\$75,000	\$125,000
Cruise Ship Infrastructure Planning / Marketing	-\$125,000	\$0
Net Cash Flow	\$0	

Projects Funded By Private Sector

Recommendations

31. Several of the projects would have the great majority of their funding by the private sector, including:

- Harbour View development
- Plywood Site Redevelopment
- Uptown Village Redevelopment
- Harbour Quay North Redevelopment

Rationale

While the public works associated with this plan are significant, the amount of dollar investment involved from the public sector under the plan is much less than the private sector investment that the plan anticipates.

The key to the success of the plan is for the City, Port and other major landowners to establish the context and related marketing to encourage appropriate private sector investment. Several of the public sector investments proposed, in addition to their public benefit, are intended as stimuli to major private investment.

Partnership Arrangements

Recommendations:

32. Two of the projects – Harbour View Village and the Plywood Site Industrial Re-investment involve land holdings of the City, the Port, and Western Forest Products. While the role of the public sector partners will be restricted to marketing the properties to others for development, there is a need for an organizational structure to undertake this marketing, in particular since lease may be a preferred form of sale.

33. The recommended partnership organization would:

- Provide an efficient means of day-to-day decisions for the partnership.
- Allow investment and returns on a pro-rata basis among the partners.
- Recognize that the City and Port have a responsibility to serve the public good.
- Legal and accounting advice should be sought on the partnership structure, which could range

from a corporate structure, to a 'trust', to a partnership agreement.

- In all cases, it is important that the terms of reference of the new organization be focused on these two projects, and avoid a duplication of decision making or effort on other matters.

Rationale:

- While there is a need for new organizations to support the partnerships, it is important for these organizations to involve as few members as possible to meet the objectives.
- The terms of reference for the organizations should clearly lay out how they make decisions, and how the organizations are wound down once their mandate is met.

34. Proposals for Uptown improvements involve the City, businesses and property owners in the area. To be successful, the projects involve both fund-raising for senior government grants, and significant co-operation among key landowners and businesses. Two organizations might facilitate the necessary changes:

- Some form of Uptown business/property owner organization would be helpful to co-ordinate and represent the joint interests of the City and Uptown landowners/merchants. Business Improvement Areas is one model, as is a 'Downtown Partnership' like in the City of Nanaimo.
- For the Uptown Village block, a small organization that includes the City, affected landowners and merchants, and related real estate advice and marketing would be an asset. In this case, the organization may be an advisory committee to Council, or a similar ad-hoc committee.

Rationale:

- Competing commercial areas like shopping centres fund full time management and marketing for the good of their tenants. These organizations also carefully manage property and support social and community improvements.
- To compete, the Uptown area needs an organization with a similar management function.

Development Sites Implementation Package & Marketing – Harbour View and Plywood Site

Recommendations:

Two of the projects – Harbour View Village and the Plywood Site Industrial Re-investment Strategy – involve the need to create a partnership and to undertake comprehensive marketing of the property for private sector investment.

If the partners are the same in both projects (e.g. the City, the Port and WFP) it may be possible to create one organization or joint venture to manage the marketing. However, if the partner organization is different, these will be similar but separate organizations.

Since the target investors are different, and the related marketing expertise would also vary, there is merit in having a separate organization for each project. However the City and Port may find there are economies of scale in having some services completed jointly for both projects.

Recommendations 35 and 36 are written as if there was a separate organization for each project.

35. For the Harbour View site, steps that the City and Port should take to support marketing to the private sector would include:

- Organization of an administrative arrangement to allow the City, Port and related private landowners to co-operate in this venture (See Partnership Arrangements above).
- Creation of a co-ordinated set of leasehold land, air and water properties that include the proposed development area. This would involve co-operation with the owners of the 'Esso' property as well as those that co-manage Harbour Road.
- Completion of base information for the development area, including topographic and underwater survey, preliminary environmental and geotechnical assessments, preliminary structural assessments, and preliminary site plan concept.
- Organization of Official Community Plan and Zoning amendments if required to facilitate the development. Development Permit requirements to guide the form of development should be drafted.
- Creation of a marketing package, related terms of reference, publicity and direct contact

with prospective investors. This might lead, eventually, to a formal call for proposals.

- Administration of an expedient review and award process related to proposals received.

36. For both the 'Industrial Reinvestment Strategy', and the 'Backup Plan', steps that the City and Port should take to market to the private sector would include:

- Organization of an administrative arrangement to allow the City, Port and related private landowners to co-operate in this venture (See Partnership Arrangements above).
- Creation of a co-ordinated set of leasehold land and water properties that include the proposed development area. This would involve co-operation with the owners of the 'APD' property and potentially neighbouring First Nations, as well as tenure over the Crown Foreshore and Port water lots.
- Completion of base information for the development area, including topographic and underwater survey, preliminary environmental and geotechnical assessments, and preliminary site plan and noise attenuation concepts if the backup plan is necessary.
- Organization of Official Community Plan and Zoning amendments if required to facilitate the development. Development Permit requirements to guide the form of development should be drafted.
- Creation of a 'MARKETING PLAN, related publicity and direct contact with prospective industrial investors (see Rationale below). This could lead to a formal proposal call for financial investment.
- Administration of an expedient review and award process related to proposals received.

Rationale:

The Port and City's goal is to have a sustainable port industrial enterprise established at the "Plywood Site". From the Port and City's perspective, a sustainable port industrial enterprise facilitates: sustainable environmental impacts, direct and indirect employment growth, and fair property tax or fee-for

service returns from the use of City and Port services, and from direct and indirect impacts on City assets.

The purpose of the proposal call process is to formulate an active business partnership between the City and a site developer.

To achieve the best result, the consultants recommend that the Port & City's approach to the proposal call be active rather than passive. An active approach entails developing potential business models, seeking potential partners, and forming a suitable partnership. The consultants do not recommend a passive proposal call process based on providing basic property information and simply awaiting the results of a broad invitation for proposals.

Work Tasks for the MARKETING PLAN

- Identifying the kinds of business operations that may be feasible;
- Identifying business strengths and weaknesses, and addressing them;
- Identifying the companies operating in the businesses being proposed;
- Engaging in a two way dialogue between individual companies and the Port/City about the development of potentially feasible business models;
- Developing the outlines of potential business partnerships between the Port/City and individual companies;
- To the Port/City's best advantage, selecting the most favourable business partner to develop the site.
- Developing a *Plan of Operations* for the Port/City to implement its obligations to the project.
- Following the *Plan of Operations* to the completion of the project.

Outline for the PLAN OF OPERATIONS

Purpose - The *Plan of Operations* is prepared by the Port/City Partnership. It is intended to be a project implementation plan for the key stakeholders for the "Plywood Site" development project. The plan is the starting point of a project.

Description - In conceptual terms, port, transport, water and land use governance institutions will be designed. Roles and responsibilities will be defined. A project *Plan of Operations*

will be prepared for an Executing Agency. The Executing Agency may be a public site development body comprising the city, the port, the developer and key stakeholders. The *Plan of Operations* will probably include the following:

PROJECT STRATEGY

INTRODUCTION

Project Background

Project Goal

Project Purpose

Associated Projects

Logical Framework Analysis

PROJECT DESCRIPTION

Location

General Description of Project

Summary of Business Case

Implementation Duration

Review of Governance Models

Design of Governance Model

Scope of Work of Executing Agency

Scope of Stakeholder Participation

Phase-out of Executing Agency

Phase-in of Governance Agencies

Key Project Assumptions

Project Evaluation Criteria

ORGANIZATION

Organization Chart

ROLES AND RESPONSIBILITIES

PROJECT CONTROL

Control Organization

Project Advisory Committee

Schedule and Responsibility for Reporting

Project Activity Network

Project Activity Descriptions

Project Implementation Schedule

- a) budget*
- b) major cost items by each stakeholder*

*Budget disbursement plan**Budget by component*

By following the process outlined, reinvestment in the Plywood site may lead to substantial community economic impact with direct and indirect skilled trades employment added.

Fishermen's Harbour Improvements

Implementation Recommendations:

37. The Port should partner with the Small Craft Harbours Division of Fisheries & Oceans Canada, as well as with Western Forest Products, to implement the Fishermen's Harbour Improvements.

Rationale:

- Land on the north side of Fishermen's Harbour is owned by Western Forest Products. WFP (Sudar) have expressed a willingness to co-operate in arranging public access on the southern edge of their property.
- Fishermen's Harbour is managed by the Port Alberni Port Authority on behalf of Small Craft Harbours, a division of Fisheries and Oceans Canada.
- It is in the interest of all concerned to maximize the effective land and water use at this site.
- Parking fees at the site might both avoid the new parking being overtaken by long-term summer users, and also might provide revenue to partially compensate WFP for use of its property on a long-term lease.
- A partnership arrangement could be struck between SCH and the Port to fund capital improvements, including the relocation of the HarbourMaster residence, new dockhead and floats, floating lookout, and associated parking, waterfront walkway and screening.

Argyle Street Phased Streetscape Improvements

Implementation Recommendations:

38. The City should proceed to construction documentation for the proposed improvements to the Argyle Streetscape.
39. Construction of the Argyle Streetscape could be completed in Phases, with a minimum of 1 block completed in each Phase. However, consideration should be given to borrowing to complete the work in a single phase.

Rationale

Improving Argyle Street will provide a stimulus for adjacent building reinvestment, and will increase confidence in the Uptown and Waterfront area.

Table 7 provides a summary of the potential cost of the envisioned improvements. The works total just under \$4M.

The works, generally, are spread over 4 blocks plus incidentals. In concept, the work could be constructed in four phases, each of about \$1M each. It is recommended that senior government co-funding be gained, to at least the level of 50% of the capital cost, leaving a cost for the City of approximately \$2M, or four phases of \$500K.

Construction costs are rising rapidly. The proposed budgets are Class D (early) estimates, but are based on unit costs that are current (2006). The earlier that projects can be funded, the lower the cumulative cost of inflation.

Detailed assumptions about the content and scope of each project can be seen in Appendix D. The calculations in Appendix D should be reviewed each year at budget time, with updates for progress and changes in unit prices and construction inflation.

Table 7 Capital Budget Summary for Argyle St. Improvements (CLASS D Estimate)

Project	Approximate Budget
Lighting	\$505K
Waterworks	343K
Storm Drainage	53K
Roadworks	543K
Wiring	435K
Irrigation	32K
Sidewalks and Hard Landscape	512K
Soft Landscape	126K
Administration and Fees (20%)	509K
Contingency (30%)	917K
Total	\$3,975K

Notes:

1. Refer to Appendix D for detailed cost estimating for these projects. Unit costs may vary.

3rd Ave. Phased Streetscape Improvements

Implementation Recommendations:

40. The City should proceed to construction documentation and construction of the 3rd Ave. Improvements, as soon as is practical.

Rationale:

- The 3rd Ave. Streetscape Improvements can proceed independently from the other projects in the strategy. Many of the 3rd street improvements may be completed as small capital projects that may be completed as convenient and funded.
- Table 8 provides a summary of the estimated costs, and a detailed estimate is provided in Appendix D. Similar to the Argyle Street paragraphs above, these estimates are Class D.

Table 8 Capital Budget Summary for 3rd Ave Improvements (CLASS D Estimate)

Project	Approximate Budget
Lighting	\$124K
Roadworks	31K
Hard Landscape	120K
Soft Landscape	42K
Administration and Fees (20%)	63K
Contingency (30%)	114K
Total	\$494K

Notes:

1. Refer to Appendix D for detailed cost estimating for these projects. Unit costs may vary.

Uptown Village Facilitation

Recommendations:

41. Proposals for major redevelopment at Uptown Village should be aggressively pursued. The City could play a facilitation role to trigger the redevelopment process.

- For the Uptown Village block, a small organization that includes the City, affected landowners and merchants, and related real estate advice and marketing should be established. In this case, the organization may be an advisory committee to Council, or a similar ad-hoc committee.
- The Organization should work with landowners in the block to jointly fund and complete a real estate redevelopment strategy. The real estate strategy would consider the concepts in this report, as well as other potential mixes of uses. The market potential for the site if it were assembled into one unit should be reviewed.
- If the real estate strategy is favorable, the partners should consider how the property could be assembled and marketed for economic reinvestment.

The City role in facilitation might include:

- Making available City property and lanes as a part of the redevelopment package. Encouraging assembly of the land into parcels suitable for redevelopment, and assisting in removing barriers to redevelopment.
- Funding, with landowner participation, the creation of architectural concepts and renderings to illustrate the site potential.
- Reviewing the current Official Community Plan and Zoning Bylaws to identify a path to ensure that the site potential could be realized. However, rezoning of the site should be withheld until there is a suitable development proposal from a qualified proponent.

Rationale:

- Co-ordinated planning for the Uptown Village block, including both public and private properties, may reveal real estate opportunities that were not evident if the properties would looked at separately.

- The size and location of this site, combined with the favorable views from the potential residential high rises, make this a unique opportunity in Port Alberni.
- Although the success of this concept is not assured, it is certainly worthy of careful feasibility study.

Harbour Quay Detailed Planning & Improvements

Recommendations:

42. A more detailed stakeholder process and financial study should be completed for the Harbour Quay Comprehensive Redevelopment Area.

Rationale:

- Existing tenants of the Port at Harbour Quay are starting to grow out of their existing premises.
- Sites in the Harbour Quay Comprehensive Redevelopment Area are underutilized at present.
- It is in the interests of the Port, the City and the private landowners to work together to devise how the Harbour Quay might gradually redevelop the underutilized properties. A key part of this work is a strategy to accommodate the operations of existing or new tenants and businesses to maximize the job opportunities and economic development that occurs.
- Existing federal interest in brownfield redevelopment might provide a vehicle to co-fund detailed planning.

Cruise Ship Infrastructure

Recommendations:

43. Investment in infrastructure that supports the Cruise Ship industry should be done when there is a broader, multi-use justification for the community.

Rationale:

- Construction of infrastructure solely in support of Cruise Ship visits may be under-utilized. It is likely that initial Cruise visits will be sporadic and concentrated initially at turn-around times at the beginning and end of the cruise season.

- If cruise-related infrastructure is funded and operated with multiple use in mind, the chance for both senior government co-funding and for economic benefits are maximized.

Shuttle Transportation Facilitation

Recommendations:

44. Funding for the Harbour Shuttle might involve an innovative approach:

- For example, partners in funding might include BC Transit, local First Nations, Human Resources Canada, Community Futures, and the local Chamber of Commerce and Tourism Associations.
- In addition, passes to the shuttle may be marketed by local businesses, in particular tourism oriented businesses, with a pass made available on all sales over \$X, with a related revenue stream from the merchant to the Shuttle partnership. In effect this would provide shuttle passes to a wide variety of visitors, and would operate like the 'U-Pass' at BC's Universities – where all students receive a pass as a part of their registration.
- In a similar vein, all sales of entry to the McLean Mill, steam train, M.V. Lady Rose, M.V. Barclay and other local attractions could include a Shuttle Pass in their package.

45. The role of the City and Port in encouraging the Harbour Shuttle should be limited to facilitation.

Rationale:

- Precedents exist for harbour shuttles that are fundamentally private sector operations e.g. the Protection Connection in Nanaimo, the Newcastle Provincial Park passenger ferry service, and the False Creek Ferries in Vancouver.
- To be economically sustainable, it is important that the Shuttle Transportation find an economic model that is not dependent on high or on-going public subsidy from the City and the Port.

The Role of Grants

Recommendations

46. The City and Port should continue to pursue federal, provincial and non-government organization grants to supplement local funding of these projects. In cases of large or discretionary facilities, in many cases the construction of the facility will be dependent on receiving approval for such grants.

Rationale

Federal and Provincial granting programs continue to evolve. Existing initiatives like the North Island Fund are not yet fully allocated, and should be pursued.

Western Economic Diversification programs may apply, such as:

- [Urban Development Agreements](#) are partnerships of federal, provincial and municipal governments working in collaboration on broad issues such as inner city revitalization, strengthened innovation or sustainable economic development.
- [Western Diversification Program](#) (WDP) invests in projects that support WD's strategic priorities of innovation, entrepreneurship and community economic development, including a number of partnership programs undertaken with other levels of government.
- [Urban Aboriginal Strategy](#), funded by Indian and Northern Affairs Canada, aims to reduce the level of disparity that urban Aboriginal people currently face by tailoring government programs to address the local needs and priorities of Aboriginal people living in cities. WD is responsible for implementation of Urban Aboriginal Strategy projects in British Columbia, Alberta and Manitoba. Service Canada delivers this initiative in Saskatchewan.
- The [Municipal Rural Infrastructure Fund](#) will invest \$278 million in the West to improve and increase the stock of core public infrastructure in areas such as water, wastewater, culture and recreation. Delivered by WD in the West, the source of funding for this program is Transport, Infrastructure and Communities.

Recent Province of BC programs include the Spirit Squares, Towns for Tomorrow and LocalMotion Funds.

There are likely other challenge grant programs that will come forward that will support senior government priorities, and many of these will apply to the Uptown and Waterfront Redevelopment Strategy.

Operating and Maintenance Requirements

Recommendations

47. New facility improvements will increase the need for operations and maintenance funding. As new facilities are completed, maintenance and operations budgets should be increased to reflect the increased costs.

Rationale

Adjustment of existing maintenance budgets to include these new demands is of critical importance.

Annual Review

Recommendations

48. Projects and priorities under this Plan should be reviewed on an annual basis, as grant application results and City / Port Financial Plans are updated.

Rationale

Although the Strategic Plan can set general directions and priorities, there will be a need for review and fine-tuning on progress in each budget cycle.

It is important that the priorities in this Strategic Plan are followed. But it is also understood that Port Alberni's rate of growth and state of opportunities will change year to year, leading to changes in the timing of various projects.

It also must be recognized that priorities beyond the +/- 10 year period of this plan will need to be considered as time goes by. For example, by 2015 there will be a need to plan investment until 2025.

8. The Strategic Vision

The Uptown and Waterfront Redevelopment Strategy sets out an agenda that will bring the Uptown and Waterfront closer to the expectations expressed by the community in the Shoreline Master Plan.

This strategy attempts to be visionary, and yet practical. It sets a clear direction, but also details the steps that lead that direction.

As this strategy is followed, there may be new paths that appear. There may be forks in the path that demand rethinking and clear decisions.

But the vision in this document provides a compass that will guide the City and Port into an exciting future:

A future where the Waterfront and the Uptown are one again.

A future that respects the past.

A future that provides an attractive home where industry, tourism and residents live comfortably and beautifully side by side.

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Appendix A

Public Input to Identify Issues

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Meeting Notes

Uptown and Waterfront Redevelopment Study – Executive meeting

Location:

Committee Room or Council Chambers
City Hall, 4850 Argyle St., Port Alberni

Date:

Thursday, **March 30**, 2006

Time:

12:00 pm - 2:00 pm

Present:

City Council: Ken McRae – Mayor, Jack McLeman, Cindy Solda, Ike Patterson, Kenn Whiteman.

City Staff: Scott Smith – Planning, Cara Foden – Planning, Guy Cicon – Engineering, Scott Kenny – Parks and Recreation, Ken Watson – City Manager, Russell Dyson – City Clerk.

Port Authority: Darryl Anderson, Jason Penner, Port Authority Board Members.

Consultant Team: Thomas Lauga – Port Planning, Russ Irish – Engineering, Richard Drdul – Transportation Eng., David Reid – Land Planning, Nigel Gray – Landscape Architecture.

Represented Parties:

Industrial Heritage Society
Maritime Society
Outlook 20/20
Uptown Merchants
Chamber of Commerce
Catalyst

Minutes:

1. Next Meeting Announcement

Issues and Ideas Workshop

Date: April 20th, 2006

Venue: Echo Centre

Time: 7:30 - 9:30 PM

2. **First Nations representatives were unable to attend this meeting. Representatives are very interested in the study.**

3. Key directives and comments from City Council:

Mayor Ken McRae – Improve access to Waterfront. First Nations Involvement. Develop a continuous waterfront walkway. Determine best use for Plywood Site, possibly non- industrial. Look at establishing a Waterfront Park and location.

Counc. McLeman – Develop and improve interface from waterfront up Argyle to Third Street. Pay attention to the existing farmer's market at Harbour Quay.

Counc. Solda – Downtown looks tired and needs updating.

Counc. Patterson – Public access to waterfront along Somass Mill is key. Look at heritage displays and land use to connect downtown and waterfront. Cable Cars? Shuttle Service? Improve signage – directional and business. Third Avenue needs a theme.

Counc. Whiteman – Incorporate wooden boat building, kayaking, sail training into harbour development.

4. Key directives and comments – General discussion:

Port Authority –

- Maintain and enhance multiple uses of waterfront.
- Preserve existing industry.
- Improve pedestrian linkage to waterfront.
- Encourage mixed-use development on waterfront.
- Improve marinas for small craft 25-28' vessels.
- Maintain 80' vessel moorage.
- Improve beautification from Third Street to waterfront.
- Improve Industrial Road – Stantec Report.
- Bring mills and forestry sector onside for interpretive, facility tours.
- Integrate 'working waterfront'. A vibrant working Port is key.
- Showcase Industry and multipurpose waterfront.
- Improve boater services and create recreational moorage tied to accommodation / destination hotels.
- Heritage pods are a good thematic idea.

- Third Street and Argyle residential / commercial mix is key. Specialty shops.
- This is a chance to 'start from scratch' and get out ahead of other similar communities.
- Provide a clear signal to potential investors. Stay true to the plan once established.
- Implementation should be based on market signals.
- Investigate potential funding options and partnerships beyond local taxpayers.
- Visiting ships are like a visiting residential community.
- Consider design guidelines for building facades – City is currently developing a bylaw.
- Expand Harbour Quay concept.
- If items are to be removed (boat launch), then provide a replacement location.

Outlook 2020 –

- Outlook 2020 has previous independent study information in PowerPoint and document form. They will provide copies to the consulting team.
- Consider a boatlift.
- Encourage 'green' building approaches.
- It is critical to start with a vision and theme for the study area. Consider a heritage theme.
- Improve physical connections for parking and access to businesses.
- Improve community safety through appropriate design.
- One public meeting is insufficient.
- Encourage a blend of waterfront uses. 'Live, work and play all in the same place'.
- Increase study area to include roundhouse. Can the steam train run out to the cruise ship dock?
- Protect view-shed from logging and clear-cutting to retain environmental aesthetic.
- Consider venues and facilities for special events hosting, i.e. Tall ships festival.
- Improve Industrial, commercial and business attractiveness.

Heritage Society –

- Include 'pods' of heritage interpretation.
- Encourage heritage tourism opportunities.
- Maritime Museum has seen 13,200 visitors in the last year – high because of the Tall Ships Festival.

City Staff –

- Pump station on Argyle at Harbour Quay. Consider flows generated by proposed future development.
- Encourage practical and environmentally sustainable practices for development.
- Port is a business specific advantage.
- Maximize Port related business opportunities with Residential and Commercial uses to follow.
- Consider festival / events services – power, water etc.
- Consider a smaller events space at Harbour Quay.
- Consider landscape treatments for vacant lots in the study area.
- Work with private property owners to improve landscape aesthetic.
- Argyle is 'way too wide'. Break up pavement. Plant trees.
- Consider railroad connections into Port.
- Include trail connections along bluff.

- Assess heritage value of existing buildings.
- Harbour Quay lights are to be replaced and should fit with proposed standards.
- Replace Harbour Quay boat launch.
- Coalmines exist along bluff – capped.
- Consider practical and cost effective maintenance.
- The Public is and has been involved in these discussions for some time.
- Provide a strategy for implementation.
- Coalesce ideas.
- Prioritize staging and phasing.

General –

- 30' boatlift is necessary at waterfront. Improve boating services and moorage.
- Cruise ship traffic is not 'if' but 'when' and needs to be considered under the study.
- Review / improve bus and traffic patterns.
- Residential and retail mix is needed.
- Beautify waterfront.
- Rock embankment from Harbour commission to Argyle is an eyesore.
- Avoid 'build and they will come' approach. Consider taxpayer impacts.
- Incorporate Residential / Commercial development opportunities.
- Narrow Argyle to make it more pedestrian friendly.
- Tie-in arts with cultural and heritage theme.
- Create a youth friendly streetscape.
- Improve Beaufort (hotel).
- Improve pedestrian linkage to waterfront.
- Encourage mixed-use development on waterfront.
- Improve streetscape by reducing hard surfaces.
- Connect waterfront and uptown with a unifying theme.
- Improve waterfront access and encourage mixed-use development.
- Provide input from R.C.M.P. to guide lighting and streetscape design. Design considering C.E.P.T.E.D. standards.

5. Question – Should there be lane Reductions on Argyle?

- Remove logging truck traffic and provide alternate route signage.
- Consider a one-way street application.
- New residents would likely favour of narrow streets, while established residents value wide streets.
- Fifth Street in Courtenay is a good example.
- Increase flexibility for business. Businesses would like to see narrow streets with more amenity space.
- A median has been tried before on Third Street with two travel lanes. The median was removed due to public concerns shortly thereafter.
- Consider a gentle curve on Argyle with curb bulges at corners?

6. Question – What is the Theme?

- Heritage – Something like Ladysmith.
- Working Port.
- Marine.
- Industrial Heritage.
- Art and Recreation
- Non-exclusionary, adaptive framework.
- City branding. Precedence studies for next meeting.
- Changing image from Industrial pollution to green development

Uptown & Waterfront Redevelopment - City of Port Alberni & Port of Port Alberni

Responses from: Issues & Ideas Workshop – Flip Chart Comments

Consensus	Issues
Renewing street develop non-restrictive theme	Big box on Third
Sidewalks, benches	Development above quay
PIEP development, redevelop harbour quay, floating pub	Dunbar & Mar, empty lot rezoning to multi-use, increase population
Third Ave. visible policing, Tax incentives for property owners	Abandon Third in favour of Argyle Development
Retail with housing above	
Argyle - develop and sell vacant properties	
Possible streetcars, tavern	
Southport - W. Walkway, information, tavern	
Plywood - marine station, dive shop, eco-tourism, artificial reef	
No median, wider sidewalks, extend harbour quay, shops around corner	
Industry should make excess land available for development	
Artisans and artists, crafts, live work	
Plywood - convention centre, residential	
Third Ave - one way designate truck route, include greenspaces	
Plywood - relocate wharf, use from quay fish plant marina	
Pull sidewalks out, remove overhead hydro	
Nighttime safety on Third Ave., lighting	
Enc. Residential/office/condos/bus use on waterfront - people downtown	
Walkway connection to Johnson	
Policing - community policing office	
Building form - Argyle Street, open to proposals	
Alternative walkway from waterfront - meandering	
Uptown revitalization task force with 90-day mandate, tax incentives, no truck traffic	
Park and street beautification	
Disabled boat access, raised concrete walkway around Southport	
Hotel Casino, boat storage at Plywood	

Uptown & Waterfront Redevelopment - City of Port Alberni & Port of Port Alberni

Responses from: Issues & Ideas Workshop – Flip Chart Comments

No high-rises	
Ext. walkway past paper mill to quay	
Plywood site - more than condos - working waterfront	
Euro. Inn on old ESSO site	
Theme for downtown core.	
Incentives for upgrades on private land	
Museum downtown	
Uptown - pedestrian friendly, off street parking	
Theme: Native (1 st Nation)	
Better signage, maps	
Argyle - greenspaces, angle parking, retain view	
Somas Hotel facelift	
Trolley bus from quay	
Small cruises only	
Eco-tourism	
Southport: need more marina and boat storage, boatel, water taxis, boat related businesses	
Plywood - low rise condo, wharf, water taxi, beach park, walkway above bank	
Uptown: empty lots as information zone, heritage, bigger shade tree canopy, median, tram	
Day liner service from Parksville	
Changing water levels	
Boat storage, working facility, boat lift	
Overland boat transport to east side	
Harbour quay retained as heritage anchor., redeveloped	
Boating community, boating tourism encouraged	
Promote route P.A. to S. Island via Forestry Road	
Colour theme, artist develop, cooperative	
Mars bombers to drop load nearer harbour	
Keep trees and cadet park	
Plywood: motor home, temporary marina facility, land boat storage	

Uptown & Waterfront Redevelopment - City of Port Alberni & Port of Port Alberni

Responses from: Issues & Ideas Workshop

21 Responses received - * is a repeated answer

1) Uptown Issues and Ideas - 3rd Ave. between Dunbar Street and Mar Street

- * No chain franchises
- * Parking off street – maybe use vacant lots
- * RCMP – more in evidence
- ** Make the downtown area more habitable. Make it a desirable place to live. Bring warmth and beauty of the core business area.
- ** Theme: Historic
- *** Better Signage & Map
- *** Mixed Use
- *** Pedestrian friendly
- *** Tax incentives to shop & land owners
- **** Lighting
- ***** Clean Streets / clean up private properties
- “Core Interest” development to draw people to area
- “English Market Town”
- “Green” median
- Absentee landlords are crippling development
- Antiques
- Arts district/artisans can live & work here
- Boutiques
- Complete overhaul
- Condos with businesses below
- Covered walkway, Mar to Argyle
- Crime, vandalism, alcohol, drugs and prostitution must be addressed.
- Divided benches to prevent sleeping on them
- Expand Parking
- Heritage artifacts / displays
- Improve transportation (roads into P.A., water traffic, airport development)
- Improved sidewalks - brick maybe
- Like 5th Ave. in Courtenay
- Logging trucks out of downtown area
- Make it different, Johnston
- Policy to discourage home businesses in residential areas but encourage setting up in the downtown area
- Provide transport linkage between Harbour Road and Third
- Public Art
- Refurbish old buildings
- Remove benches to eliminate loitering
- Road access linking P.A. to Island Hwy over Horn Lake Road
- Sidewalk café's
- Small cottage industries
- Taller planters so seating is not an option
- Theatre, Dinner, Evenings
- Theme: Native
- Two seat benches for people who have to rest – limits loitering and group gatherings
- What can be done about the Beaufort Hotel? It is an eyesore

2) Uptown / Waterfront Link Issues and Ideas - Argyle Street between Harbour Road and 5th Ave.

- ** Greenspaces for pedestrians & seating to rest & view harbour - to get to uptown destination.
- * Improve median - Argyle below 3rd - make it more inviting
- ** Remove power lines to improve view of Arrowsmith
- * Street cars/trolley buses from quay
- ** Businesses needs facelift, more attractive
- Airport improvements
- Develop Harbour Area
- Development of harbour view site – condos
- Downtown nucleus should be Argyle, 1st, 2nd and Kingsway. Already empty lots, parking potential, & park
- Eco tourism
- Empty lots on 1st and 2nd – potential building sites
- Encourage small cruise line only
- Ferry Link (from Victoria or Seattle)
- Harbour view Site – leave it alone or make it a park
- Hotels – refurbish or develop new ones
- Hotel – 5 star tied into marina
- Keep trees
- Landscape the hill-climb, stairways, rest benches, viewpoints.
- Landscaping should feature native plants.
- Link all of the good looking/successful business to one another via walkways/cycling
- Low rises
- Maintain area for future industrial development
- Move Alberni engineering / fish plant to this area
- Pedestrian friendly
- Promote commercial/residential - mixed use
- Remove one of the liquor stores
- Roundabout at 2nd and Argyle with a garden in the middle
- Scooter rental
- Seafood Restaurant
- Sidewalk café's
- * Somass's to make a 1st class hotel/dining, view of harbour
- Theme: reflect community, native, industrial, fisheries, agriculture, sports. (we have the only drive-in restaurants on the island).
- Trees on the hill – leave them
- * Tavern or floating pub with small boat access
- Total redevelopment of area under a theme that ties in all sites

3) Southport Waterfront Issues and Ideas - south side Somass Mill to north side A.P.D. Mill

- * Marine Pub
- **** More boat marinas
- *** More boat storage
- **** Walkway/Cycleway (waterfront)
- Artificial Beach
- Better signage
- Boat repair
- Boat ramps (wider than present)
- Condos on hillside between the trees and the waterfront
- * Fishing pier - extended out into harbour
- Harbour quay – reinvent it, include a farmers market
- Information access
- Keep market more friendly - covered stalls
- Liked the idea of covered boat storage + condo
- Low-rises

Mixed Use

No Large cruise ships

Picnic Areas

* Piers

Promote small business, boat related, kayaking, wind surfing

* Rail link from Qualicum or Parksville

Remove Water Street wharf

Restaurant

High-end residences may complain about noises from trucks & smells from fish plant.

* Travel-life installed on the waterfront for boaters

Trees along roadways

Walkway against/inline with rail line and accessing uptown streets

Walkway below park bank

* Walkway from Harbour quay to Maritime heritage centre

Walkway all the way between the 2 mills

Water taxi to link dike

4) Old Alberni Plywood Site Issues and Ideas

** Eco tourism

* Involve 1st Nations in development of Plywood

* Low-rise condo

* New Hotel

**** Additional marina

** Extended walkway along the water

** Man made Beach

* Boat storage facility

Billboards must go

** Condo's that fit the local character (a west coast fishing village) – with businesses below/greenspace/boats & float planes tied up in front

Cut into the bank for walkway - terrific views

Drydock for boats

Fishing Pier

* Greenspaces

Housing potential if covenant lifted

Industrial traffic needs to be separated from other vehicles

Kayak and small boat rental

Linear park along waterfront

Maintain area for future industrial development

Marine Info Station/centre (small scale)

Move fish plant

* Multi-use (residential and small business and restaurants)

New wharf

* Park

Pedestrian friendly

Plywood site: site contamination?

Pub

Reefs

Seaplane base

Trees

Travel-lift (for boats) site potential

Water Taxi connection to use harbour

Wrecking yard – is useful but build a fence and have planting to hide it

Appendix B

Public Review of Alternatives

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Uptown & Waterfront Redevelopment – City of Port Alberni & Port of Port Alberni

Alternatives Workshop Response Form [INDIVIDUAL] **34 FORMS SUBMITTED**

The City and Port Authority are planning together for the future redevelopment of the Uptown Area and portions of the waterfront between the Somass Mill and the former Alberni Plywood Site. At this point in the process, the consultants are considering a range of alternatives, varying from moderate to significant investment. Funding for improvements may be some time in coming. Other than streetworks, the focus of the City and the Port will be to set the right conditions to encourage private or donor investment, as opposed to funding all major projects themselves.

What aspects of the Alternatives do you support? Both the public input received and technical analysis from the Consultants will be submitted to City Council and the Port of Port Alberni as a part of recommendations.

1. What improved linkage between waterfront and Uptown do you support, on Argyle St. between 4th Ave. & Kingsway?

Both options presented include several features in common. The options vary in the amount of median landscape, the number of travel lanes, and shuttle services. Which elements do you prefer?

Both Options: (CHECK ALL THAT YOU SUPPORT)

- | | |
|---|-----|
| A. Retain existing angle parking | 53% |
| B. Add curb bulges at intersections | 56% |
| C. New and wider sidewalks (width varies) | 56% |
| D. Underground wires | 74% |
| E. Encourage redevelopment of vacant City properties to mixed retail / office / residential | 76% |

Options (CHOOSE OPTION A OR B)

Option A, with 4 travelled lanes of traffic, and improved BC Transit service between Harbour Quay and 3rd Ave. **26%**

OR

Option B, with 2 travelled lanes of traffic, plus central landscaped median, and seasonal / special occasion rubber tired shuttle **62%**

2. How should maritime and industrial heritage be showcased?

The public expressed interest in an adaptable theme – and the maritime and industrial heritage of the City could be a key candidate. Each of the two options has different proposals to showcase this heritage. Which of these do you prefer? (CHECK ALL THAT YOU SUPPORT)

Option A:

- | | |
|--|-----|
| Install maritime and heritage artifacts where sight distance allows on intersection curb bulges. | 74% |
| Arrange for guided tours of local industries and the waterfront. | 53% |

Option B:

Create professional DVDs and videos that are virtual tours of local industries, people and related markets. Show these in unmanned kiosks along the street to create a series of 'attractions' spaced optimally. Also make versions for sale in local stores. Incorporate the video screens into customized pieces of art spaced along Argyle Street. **9%**

3. What actions to showcase environmental sustainability do you support?

Both options include actions to improve the environment of Uptown and the waterfront. Which of these do you prefer?
(CHECK ALL THAT YOU SUPPORT)

Option A:

Increase tree planting. 65%

Install interpretive signage to highlight the 'cleanup' of local industries over the last 25 years. 26%

Use 'Green Shore' techniques as a part of any waterfront redevelopment. 53%

Option B:

Use separated stormwater runoff to feed 'infiltration swales and rain gardens' in the Argyle medians or boulevards. 'Celebrate the Rain'. 47%

Consolidate and increase the amount of green space and high-value uses on the waterfront, with consideration of alternate locations for lower value uses like surface parking. 41%

Encourage improvement to waterfront water quality during redevelopment. 56%

4. What improvements to appearance and function of 3rd Ave. do you support?

Along 3rd Ave., the Options have several items in common, and key differentiating features. However, many of these elements can be mixed and matched. Which improvements do you prefer? (CHECK ALL THAT YOU SUPPORT)

Both Options:

A. Planted median to painted line areas north of Argyle 56%

Roadside improvements, including:

B. Replace lighting with new decorative fixtures 68%

C. Repair crosswalk brick paving and sidewalks 59%

D. Renew shrub and flower plantings in planters 65%

E. Improve street signage 71%

F. Add better street furniture (benches, litter containers) 68%

Option A

Screen off-street parking areas with planted hedge or decorative fence. 38%

Option B

Replace existing wood bench cap on stone planters with concrete (or stone) cap and anti-skating devices. 41%

Encourage a Business Improvement Area (BIA) to start programs to increase marketing and improvement of private properties. 41%

5. What improvements to land use and social conditions of 3rd Ave. do you support?

The public has expressed some concern about social issues on 3rd Ave. While this is a land use study, not a social study, there are some initiatives that may be helpful. (CHECK ALL THAT YOU SUPPORT)

Both Options:

Encourage a street outreach program by merchants to provide information and alternatives to those in need. 38%

Option A:

Increase enforcement of existing or new bylaws. 35%

Option B:

Review land use policies to encourage market and/or social housing and addiction treatment services as part of the redevelopment of the Beaufort lands (e.g. allow work/live units along the street frontage, and public parking behind under a office-like treatment centre with housing above). 38%

6. How do you think pedestrian crossing of Harbour Road at Arygle could be improved?

Pedestrian crossing of Harbour Road at Arygle has been less comfortable than ideal. To reduce this barrier to pedestrian connectivity:

Both Options: (CHECK ALL THAT YOU SUPPORT)

A. Install improved materials and curb bulges to ease pedestrian crossing of the railway tracks, while recognizing turning movement requirements of large trucks. 53%

B. Improve pedestrian access through the Railway Station property. 59%

Options (CHOOSE OPTION A OR B)

Option A:

Reconfigure the access to Bird Ave. to increase pedestrian comfort and safety. 38%

OR

Option B:

Investigate a land exchange or sale to re-orient Bird Ave. away from this intersection, and encourage private redevelopment of underused properties in this area. 47%

7. How can we balance community uses and economic development potential for the Sea Cadet property?

Existing trees on the Sea Cadet property (corner of Harbour Road and Argyle) are valued by the community, and at the same time there is economic development potential for this water view property.

Both Options: (CHECK ALL THAT YOU SUPPORT)

- A. Retain the 'core' of existing fir trees at the hilltop. 56%
- B. Improve the central treed open space, parking and street frontage area as a high-amenity public area and parking. Some of the existing parking would be used by adjacent condo occupants. 41%

Options (CHOOSE OPTION A OR B)

Option A: Consider limited development of the north edge of the property, for retail along Argyle and residential above (may impact up to 4 trees). Parking for this may include part of the existing surface parking on the site's east side. Consider limited development of the south edge of the property, for residential use with parking under (may impact up to 3 trees). 24%

OR

Option B:

Provide 'special event' and access improvements between Argyle and the treed area e.g. bandstand, gazebo, stair, seating slope, etc. Consider limited development of the south edge of the property, for residential use with parking under (may impact up to 3 trees). Extend this housing development on concrete deck over part of Harbour Road and the railway, with a pedestrian spine / fire access gently sloping down to join the waterfront trail at Harbour Quay, with Harbour Road and parking under. West of Harbour Road, provide an enclosed 'heritage rail work area' under the overhead deck, and allow continuous trackage so that heritage trains can be displayed along the existing tracks to the south of the 'work area'. 56%

8. What changes to the Water Street Wharf / Esso Property area do you prefer?

The Water Street Wharf is near the end of its life span. The Esso private property is environmentally constrained, and may be a candidate for non-residential uses?
(CHOOSE OPTION A OR B BELOW)

Option A:

Remove and perhaps replace Water Street Wharf in a new configuration, approximately at its current location. Consider provisions for tie ups of visiting vessels at both wharfage and floats. Include a waterfront walk that crosses behind the new Water Street Wharf to extend along the Esso property to join to existing walks at the Harbour Quay Marina. Improve the waterfront of the Esso property for public use. Encourage private sector housing (with under-building parking), waterfront commercial and marina in the former Wharf area 29%

OR

Option B:

Develop a new waterfront walk on fill / piles that connects Harbour Quay along the front of the proposed housing, through the old Esso property above the existing wall and connecting to the existing Marina walkways. Improve the waterfront of the Esso property for public use. Encourage private development of a new marina to replace Water St. Wharf and the water between it and the existing Harbourfront Marina. Part of the moorage at this marina would be made available to the upland condo owners. Outer floats at this marina would be designed for larger boats, and for special event moorage. 47%

9. Do you support the 'Large Cruise Ship Infrastructure' that is proposed?

Pocket size cruise ships could be handled by existing or new wharfage. To increase the attractiveness of Port Alberni to large cruise ship companies, a limited amount of infrastructure improvements are required, in particular to support customs clearance and large numbers of guests.

Both Options: (CHECK ALL THAT YOU SUPPORT)

A. Provide an attractive pedestrian gate system near the Maritime Gallery to manage customs clearance, and install attractive outdoor displays and landscape to welcome Cruise Ship pedestrians onto the waterfront walkway system. Consider a new 'customs' facility near the Maritime Gallery, if demand warrants (may be a temporary structure initially). 44%

B. Adapt existing floats in the Harbour Quay Marina to accept 'tender' transport from anchored Cruise ships. 32%

Options: (CHOOSE OPTION A OR B)

Option A:

Use the existing Harbour Quay Marina parking for guest pickup by taxi or small vehicle. Allow (with inspection and permit) buses and large tour vehicles to use the existing secure Terminal loading area to pickup and drop off cruise ship passengers. 26%

OR

Option B:

Extend the existing Harbour Quay Marina Parking Area to the south, into a corner of the existing Terminal area, to provide additional parking as well as turning radius and parking for buses and large vehicles. Create attractive security fencing between this transit exchange and the Terminal area. 35%

10. What land uses at the 'Plywood' site south of A.P.D. Mill do you support?

The site of the former Plywood Mill is now owned by the City, with waterfront areas under Crown and Port control. An existing covenant runs with the land, that prohibits the use of the site for overnight accommodation. The feasibility of the options below that include residential uses would require the covenant to be removed. (CHOOSE EITHER OPTION A OR B)

Option A: ☐ (IF YOU CHOSE OPTION A, THEN CHECK WHAT ELEMENTS OF OPTION A YOU SUPPORT)

A. Assemble, market and sell land / water lot for a new heavy industry e.g. a Specialty Sawmill or redevelopment of APD. Waterfront would be log storage and/or industrial marine access. Access to the site would be Plywood Drive and/or through APD. Rail access may be possible through APD if tracks were re-established. Upper terrace could be employee parking / site office and/or product storage **12%**

B. Public access to a waterfront viewpoint or surplus waterfront lands could be negotiated as part of the land sale. **9%**

OR

Option B: ☐ (IF YOU CHOSE OPTION B, THEN CHECK WHAT ELEMENTS OF OPTION B YOU SUPPORT)

A. Assemble, market and sell land for a 'boat storage and service' cluster along the northern section of this waterfront. Choose one of two sub-options: B1: Small Craft Stacked Storage Bldg. / Service **38%** OR

B2: Large Craft Ship Yard with Vessel Lift **35%**

C. Develop public parking, boat ramp and a public seawalk along the central and southern edges of the property. **35%**

D. Redevelop the 'pier' to act as equivalent to Water Street Wharf. Improve truck and float provisions for small craft (FN) fishery landing. Provide floats and provisions for visiting large and small craft. Consider leasing pier or adjacent space for a waterfront restaurant or pub, or dive shop. **35%**

E. Market condos stepping down over the shoreline slope at the south end of the site. Habitable areas should be above 10m for tsunami protection. Market water lots for a private marina development – serving both the upland condos and the public. Could be also marina development to south in association with First Nations. **44%**

11. What gradual redevelopment of the Harbour Quay and Fishermen's Wharf area is appropriate?

Harbour Quay is a prime destination for tourism. Can it be improved? The Fishermen's Harbour is well established, but congested. Please check the improvements that you support: (CHECK ALL THAT YOU SUPPORT)

Option A:

Limit public sector involvement, but encourage private redevelopment of Harbour Quay as market allows. 24%

Provided that continuous waterfront public access remains, and replacement of playground elsewhere on the waterfront, and movement of farmer's market to 3rd Ave. area, consider limited private development of existing playground and parking area at Harbour Quay to a higher use e.g. ground floor commercial and second floor housing. 15%

Option B:

A. On the north side of Harbour Quay and around Fishermen's Wharf, provide public sector planning to support land exchange or purchase to create more viable parcels for private redevelopment as market allows. For example, consider how the parcels between the Somass Mill and the Lady Rose could be reconfigured to best support gradual private sector improvements and complementary marine-oriented uses. 41%

B. Pursue agreement with Somass Mill to allow a public boardwalk and limited associated public parking along the south edge of its property (fronting the Fishermen's Harbour). 59%

Comments or Other Suggestions

REFER TO SEPARATE FILE

Thank you for your time and thoughtfulness. Kindly hand in your individual response form at the workshop, or return it by end of business on July 6, 2006 to:

City of Port Alberni
4850 Argyle Street, Port Alberni, BC
ATTN: Scott Smith, MCIP, City Planner

If you require further information, please contact:
Scott Smith, MCIP, City Planner, City of Port Alberni (250) 720-2808

Uptown & Waterfront Redevelopment – City of Port Alberni & Port of Port Alberni

Alternatives Workshop Response Form [INDIVIDUAL]

The City and Port Authority are planning together for the future redevelopment of the Uptown Area and portions of the waterfront between the Somass Mill and the former Alberni Plywood Site. At this point in the process, the consultants are considering a range of alternatives, varying from moderate to significant investment. Funding for improvements may be some time in coming. Other than streetworks, the focus of the City and the Port will be to set the right conditions to encourage private or donor investment, as opposed to funding all major projects themselves.

What aspects of the Alternatives do you support? Both the public input received and technical analysis from the Consultants will be submitted to City Council and the Port of Port Alberni as a part of recommendations.

Form 1:

I prefer Harbour Quay as park and farmer's market.

Form 2:

No comments.

Form 4:

Q.1 – Both Options: Yes. May not be cost effective. Not Sea Cadet Park – see comments 2 & 3.

Q.1 – Option A: If cost effective.

Q.1 – Option B: No.

Q.2 – Option A: Industry already does this when they can afford to. And in Sea Cadet Park.

Q.2 – Option B: "in local stores" Ok. "video screens" No. They will be vandalized.

Q.3 – Option A: "cleanup" of local industries" We must be careful not to suggest anti-industry sentiment. We have lost too many jobs over the years. Better to highlight how we can co-exist with industry.

Q.3 – Option B: "celebrate the rain" No. It is a mistake to think of parking as 'lower value'. If people can't park they won't stay. (Let's learn from Wal-Mart). "waterfront water quality" Sounds a bit anti-industry.

Q.4 - "replace lighting" Not the ones like at Zellers. Leave them alone. Ok ideas but at what cost? Do only the most cost effective ones first. "BIA" Yes but carefully – Keep the devil out of the details – involve the public in the reasoning.

Q.5 – Both Options: Ask the merchant.

Q.5 – Option A: Yes, but mainly needed in relation to misbehaviour such as littering, graffiti, vandalism, etc.

Q.5 – Option B: More parking is needed for the sake of small businesses. There exists a parking lot on 2nd Ave between Anguson and MAR which badly needs a connecting breezeway to 3rd Ave. Addiction treatment? – Let's not get carried away.

Q.6 - There is little difficulty walking east-west because north-south vehicles are controlled by a 2-way stop. But east-west vehicle traffic finds the intersection confusing, especially at shift change. There should be a 4-way stop. "improve material and curb bulges" Not necessary. "improve pedestrian access" Hardly necessary. "reconfigure the access the Bird Ave." Not necessary. "Option B" No.

Q.7 – Both Options: A. No. Retain and improve the park as it is now. B. No.

Q.7 – Option A: Bad ideas – The north edge is already the most popular sitting spot. Also, a building there would spoil the view to the north from the rest of the area. Residences near the bluff not desirable from noise consideration – see comment 3.

Q.7 – Option B: Use the park for industrial heritage displays, Special Events Venues and for a War Memorial (Navla and Merchant Marine).

Q.8 – Option A: "remove and perhaps replace wharf" If it is not unsafe, perhaps it is ok as it is. "encourage private sector housing" No, see comment 3.

Q.9 – We have handles "the world" with what we have. Let's not get too specialized.

Q.9 – Both Options: "adapt" Would 'adaption' be needed?

Q.10 – Hard to say. It may have to be developed piecemeal. And leased, not sold. Option B – B2: Good idea.

Q.11 – Option A: “limit public sector involvement” Yes. “farmer’s market to 3rd” No. “housing” No, see comment 3.

Comments or suggestions:

1. Money is tight – let’s not waste it.
2. Sea Cadet Park is already in good conformance with the 1991 Port Alberni Shoreline Master Plan (Taylor Peach & Associates). Let’s keep it as it is, formalize it as a park and put it to use as a tourist attraction.
3. Recognize that industry and tourism can be compatible but industry and residency leads to complaints and eventual loss of the industry. We need to avoid siting residency where it will generate noise complaints etc. The bluff edge will be noisy as long as there is industry on the waterfront. But the bluff acts to deflect noise over the water – a great advantage for buildings located well upland from it.
4. Port Alberni owes its very existence to its harbour and its harbour related industry. Our future prospects will be greatly diminished if we don’t keep a working harbour.

Form 5:

Q.1 – Both Options: Leave the Sea Cadet property as is!! Dennis Jonson site sold already to be mixed use.

Q.1 – Option A: Only if affordable.

Q.1 – Option B: No.

Q.2 – Option A: “arrange for guided tours” Should be encouraged – some already happening, I believe.

Q.2 – Option B: “kiosks” No. Will be vandalized. “professional DVD’s” Yes. DVD’s and videos – good idea – possibly sell at Tourist Bureau and local businesses.

Q.4 – “lighting” Current lighting is fine – waste of money to change this at this time. “repair paving” Where needed. “street furniture” If affordable. “BIA” Is there not a Downtown Business Association already – Don’t really know what BIA is – can’t comment.

Q.5 – Both Options: Not the responsibility of merchants.

Q.5 – Beaufort Lands are privately owned at this time.

Q.6 – Don’t feel this is a serious problem at this time. “improve pedestrian access” No.

Q.7 – Both Options: No – Keep the entire property as is – See comments.

Q.9 – “pocket size cruise ships” Minimal facilities to accommodate this.

Q.10 – “large craft ship yard” Yes. Land could be leased for various uses.

Comments or other suggestions:

1. Sea Cadet site is a valuable asset – for both locals and tourists. Should be enhanced and designated an official park. There are other sites to build residences. People living in those residences would be able to use the park along with all other citizens and tourists. The Sea Cadet site, Harbour Quay, Train Station all fit together nicely as a central tourist area. The site needs additional shrubs, plantings, lighting, picnic benches, gazebo with a view – site – could be a service group funding? Put industrial and maritime heritage artifacts with plaques and information on the site and retain necessary parking.
2. Until 3rd Ave and Uptown vandalism is addressed – and the state of the Beaufort Hotel – why spend funds on new lighting, street kiosks, etc.
3. Somass Hotel is for sale – could be a beautiful old hotel if sold and monies spent on reviving it as a proper tourist hotel – or condos.

Form 6:

Q.1 – “vacant city properties” ie: ‘brown spaces’ only there are a lot of them; no ‘development’ of existing green spaces.

Q.1 – Option A: “improve transit service” Not enough traffic to sustain transit route in Winter. Not enough traffic to justify 4 lanes (and narrower sidewalks).

Q.2 – Option A: These kiosks, under present conditions, would probably be vandalized within a week of installation – probably beyond repair. “arrange for guided tours” Advertise existing operators.

Q.4 – Both Options: “planted median” No. Visibility is already a problem at the low end. “replace lighting” No. Existing fixtures are unique – change globes to “break-proof” plexiglass (re: vandalism). “renew plantings” Switch to native perennials with info plaques to increase uniqueness and reduce costs. “signage” Keep it simple. “street furniture” All benches to be divided (as in almost all other cities currently) to discourage vagrancy (also helps seniors getting up).

Q.4 – Option B: See above re: divided benches. “BIA” Find ways to assist local businesses to buy commercial buildings to gradually reduce out-of-town ownership of such properties. Out-of-town owners have been consistent in spending an absolute minimum to maintain properties here.

Q.5 – Both Options: No. Merchants pay taxes and business licenses to support such initiatives – not their responsibility.

Q.5 – Option A: Get RCMP out on foot-patrols in uptown area.

Q.5 – Option B: Beaufort Hotel lands must be brought back under city umbrella.

Q.6 – Option B is simply too costly.

Q.7 – Both Options: “trees at hilltop” Retain all. Eliminating perimeter trees will make remaining trees vulnerable to blowdown. “existing parking” Carm--- block only.

Q. 7 – Option A: No. See above. As the city grows. The already small size of this ----- but undeclared ‘park’ will become even more obvious.

Q. 7 – Option B: Much too expensive. Vibration of constant heavy traffic will challenge structural integrity. No – south edge is windward edge of this highly exposed bluff. Elimination of guard trees will make exposed trees vulnerable to blowdown.

“bandstand, gazebo, stair” Utilize already cleared areas only.

Q.8 – Option A: “private sector housing” not in inundation zone. “under-building parking” This would be submarine level and require constant pumping.

Q.9 – Both options: “attractive outdoor displays” All such improvements would benefit local users year-round.

Q.9 – Option B: Extra public area here could benefit the whole community for major waterfront events, regardless of cruise ship.

Q.10 – Option B: “tsunami protection” Inundation zone map cites more than 10m. ie: intersection of 3rd and Argyle is just outside inundation.

Q.11 – Option A: No. New residential development within inundation zone.

Comments or other suggestions:

No improvements will last long in “uptown” until the issues of vandalism, property crime including shoplifting, vagrancy, drugs, alcohol and prostitution are addressed and resolved. A key element here is the (re) introduction of RCMP foot patrols and visible street presence to make the area less attractive to these elements.

Enhancing the walk up Argyle (even if it escapes vandalism) may have limited impact. Visitors (and locals) simply do not feel comfortable walking up Argyle through the ‘cliente’ spilling out of the Somass and King Edward Beer Parlours.

A formal commitment to recover all derelict lots into city, ownership, or to “encourage” owners to cleanup (at least) or develop (better) such lands. No green space rehabilitated and/or developed.

1. Regarding construction in a known hazard area (ie: inundation zone):

A reputable local insurance agency has indicated that all new construction may first be “vetted” by insurers. Construction on a known unstable base is unlikely to be allowed to proceed.

2. A check of standard insurance policies reveals that certain types of damage are not insurable (the one exception is earthquake coverage, which can be purchased as an add-on). Not insurable are loss or damage:

“13) caused by snowside, earthquake, landslide or any other earth movement. If any of these results in fire or explosion, we will pay only the resulting loss or damage;

14) caused by settling, expansion, contraction, moving, bulging, buckling or cracking, except resulting damage to building glass.”

Water damage not covered includes:

“17) ii – caused by continuous or repeated seepage or leakage of water

iv - caused by groundwater or rising of the water table

v – caused by surface waters including flood, unless the water escapes from a watermain or from a domestic water container located outside your dwelling.”

Reference to ING Insurance Homeowners Comprehensive Policy. My emphasis.

4. In the recent case of homes lost to landslide in the Vancouver area, the municipality which permitted construction is now facing homeowner claims, since the loss is uninsurable.

Form 8:

Q.7 – Both Options: “trees at hilltop” Yes. Make it a park.

Comments or other suggestions:

Please get the "Sea Cadet Park" with our own mini Cathedral Grove designated a park for all time. Maybe a few benches and litter cans for convenience but basically leave it as it is – beautiful. Shows our history – mature trees are important to leave. Please.

Form 9:

Q.4 – "BIA" Or some type of other model (society).

Comments or other suggestions:

Item #9

No significant capital expenditures should be made on customs facilities until community economic benefits and revenues can be justified. If built, such structures should not negatively reflect on existing buildings and the land they presently occupy. I.e: Maritime Discovery Centre Gallery Buildings.

The "small ship" (250 Pass and under) market demand and product assessment for the BC Coast has been well researched. This market may prove to be more beneficial and appropriate for Port Alberni. Resulting in far less need for costly waterfront infrastructure.

Form 10:

Q.1 – Both Options: Focus on upgrading uptown – principally 3rd Ave.

Q.3 – "interpretive signage" Do not threaten existing marine businesses.

Q.4 – Both Options: Very little effect if you do not draw traffic to 3rd. What are your tourist signage plans? What about advertising to get people to spend or week with use - & go to West Coast as a day trip.

Q.7 – High end condos will threaten all the wharf jobs that already exist. By petition and at the public meeting, the public overwhelmingly refused to privatize any part of the Sea Cadet park.

Q.7 – Both Options, Option A: No to condos.

Q.7 – Option B: No to any privatization.

Q.11 – Option A: Vague – another give-away of publicly valued property.

Comments or other suggestions:

To the entire Council. Who are you serving? Certainly not the citizens of P.A. This report flies in the face of existing industries on our wharf. The principle that our waterfront be handed over to private developers, should be honestly discussed at well advertised public meetings. This is not a redevelopment of uptown plan, but a thinly-veiled sell-off of prize pieces of waterfront to high end users. I object to the under advertised and ill timed discussion of this report (June 28 just before a long weekend and start of summer, visitors, gardens demanding prior attention). Have the courage to open this in a meaningful way to all the citizens of Port Alberni. This is our home. We all have an interest. The ads should not be buried on the sports page, with very little prep time. Give us an open public meeting – not break us down into individuals who must have the skills of Sherlock Holmes to keep up with what is really going on. In conclusion Shame! Shame! Shame! For the sneaky way this has been done. J. A Jelinek
Spain's Mediterranean coast has been destroyed by high end condos – now tourists are going to more "natural destinations" in the Adriatic.

Form 11:

Recommend to merchants that they repaint storefronts in bright, coordinated colours.

Trees are pivotal – median with trees helps provide shade in bright sun. I.e: look at street chestnuts in Victoria.

Leave the farmers market at the Quay. Brings people to an area with a variety of shops, water, park, etc.

Residential and commercial is the best ---.

Pedestrian friendly – less parking, more people. Bus service, possibly encourage tiny tourist ferry bunts (like in Victoria and Granville Island) shuttling to the Victoria Quay area and to tour shore attractions (welcome figures, etc.)

Form 12:

Some minor area good ideas.

1. Does not deal with the political issues involved with the Port Authority, City and Regional District.
2. Seems to be bits and pieces but no cohesive plans.
3. Without a comprehensive recreational boat servicing including a travel lift – the recreational boaters with larger boats will not come.
4. Seems the mandate in terms of reference were too limiting.
5. The assembly wharf area seems excluded, why?

The city will say it can't afford it.

The Port Authority will say there is no clear direction.
So nothing will change.

Form 13:

Q.1 – Both Options: “retain existing angle parking” Yes, but would this be safe enough and efficient enough if there are only 2 traveled lanes. “encourage redevelopment” And/or could some of these could become rest stops with historical artifacts and interpretation, benches, etc. Perhaps some could house community gardens, near to where the gardeners live.

Q.1 - Option B: See above re: angle parking.

Q.2 – Consider moving the AV Museum down to Harbour Quay, Argyle Street or to the present Zeller’s bldg (!)

Q.2 – Option A: “heritage artifacts” And perhaps in a few presently vacant city-owned properties. “guided tours” Incorporate the maritime heritage venues as part of this.

Q.2 – Option B: I don’t like this option at all. Having such videos for sale at tourist info bureau, museum, mill etc. would be fine, but I think that real “attractions” – locally produced public art, industrial heritage/maritime artifacts with interpretive plaques like those along the harbour would be better.

Q.3 – “interpretive signage” Include these only as part of the historical “sights”!

Q.3 – Option B: “celebrate the rain” Great idea! Flow-forms, rain gardens – yes! “surface parking” The present on-site parking and turnaround are well-used and attractive especially to elders and handicapped. Some buildings are underused and could be redeveloped.

Q.4 – Both Options: “street signage” Using signage in similar style and colours – easier on the eye!! Make it easier for tourists to find the attractions. ie: Capitol Theatre. “street furniture” Or spruce up what we have already.

Q.4 – “BIA” Is there not already such a group, though not perhaps by this name?

Q.4 – I was surprised at last week’s public meeting to realize that the input/suggestions of those living and running businesses in the subject areas of town had not been directly solicited before the process go so far. I strongly suggest that this be done now, thereby saving time and effort, and gaining the support of those affected.

80 to 90 people is a good turnout to meetings, but some key players may still need to be heard from. Let’s at least check that they are

Q.5 – Both Options: “street outreach program” This would take some training, maybe by community policing. I wonder if all we citizens shouldn’t be recognizing and directly addressing those in need on the streets – recognizing that they too are our fellow citizens and trying to at least offer a smile and understanding. That would make a big difference.

Q.5 – Option A: Can we afford police “on the beat”?

Q.5 – Option B: I think this is an interesting and worthwhile option to consider. It begins the embracing and recognition of all citizens, as I mentioned above.

Q.6 – Both Options: “install improved materials” There are already considerable curb bulges but they appear to meld into the roadway; perhaps the edges could be brightly painted and raised a little. “improve pedestrian access” Perhaps through one of the former cargo bays – if attractively finished this could entice pedestrians to go farther a field.

Q.7 – Both Options: “trees at hilltop” Definitely and without doubt!

Q.7 – Option B: This option retains or improves the grassy hill on Argyle St. and also gives other spots of access; I believe that visually and in terms of enhanced public access/enjoyment, this is a far better option.

Q.9 – I do not support the attracting of / building for large cruise ships, only pocket size ie: eco-tour ships. The spending and disruption will not be compensated for or recovered, as your own Port Planner told us at the meeting. Only those infrastructures necessary for pocket size cruise ships should be undertaken.

Q.10 – Option A: will current citizen/industry/government discussions open new possibilities soon? How long can we wait to make this decision? Is this really a practical realistic option? Is there a foreseeable market?

Q.10 – Option B: “B1” Travel lifts needed and private vessel repair (etc) shops for small craft.

Comments or other suggestions:

Re: 3rd Ave and Argyle Street, there were suggestions at the first public workshop that a complementary colour scheme for adjacent buildings be encouraged to draw businesses together and pedestrians along the street. The storefronts on the north side of Argyle at 3rd are very attractively refurbished, in harmony of colour, but retaining their unique characters. This is very attractive to residents, passers-by and tourists.

Could the city recognize these merchants’ efforts – even with just a quick call, pop-in visit, brief note? How could other merchants and businesses be encouraged to brighten up their premises?

Thanks for listening.

How can we ensure that as many people as possible, especially those living and working downtown, read these options and offer suggestions? Perhaps all that can be done has been done to solicit input and I have just been sleeping.

Form 14:

There needs to be a well thought out plan that is affordable and that can be done in the near future. (ie: waterfront walkway from Harbour Quay to China Cr.). A revitalization of 3rd Ave. trees, planters, green space. A miniature Granville Island. Encourage developers to invest in this community.

Form 15:

Q.10 – Option A: Extend rail export for outward bound destinations, less traffic on highway.

Comments or other suggestions:

We have over the years had many consultants make their decisions with help of the community. I believe there should be no more spending on consultants and apply the money to some of these projects listed. Marketing Port Alberni is the key element that will bring some of these dreams to reality. Money is a big issue and investors are what we need. A board of such investment opportunities should be created to attract these kinds of people. Good luck.

Form 16:

Q.1 – Both Options: “underground wires” Explore co-funding options with BC Hydro/Telus.

Q.1 – Option B: Absolutely! Ensure small L-turn bays at intersections. Alleviate concerns some would have with traffic congestion on those rare occasions!

Q.2 – Option A: “heritage artifacts” Okay. “guided tours” Maybe during special events.

Q.3 – Option A: “tree planting” In exchange for those harvested as necessary for development projects. “interpretive signage” No. Why remind people of past “misgivings”?

Q.4 – Both Options: “planted median” Absolutely. “street signage” To signs large enough to read and added for directional info to attractions/districts. Re. Sign strategy implementation?

Q. 4 – Option A: No. Only gives place/cover to hide – re: activities.

Q.4 – “BIA” Work in local stakeholder agencies to partner and support.

Q.6 – I’ve never felt this a problem no ----- whole park or walk to/from! These people with issues should visit VCR, Seattle, Victoria for perspective!

Q.7 – Both Options: “trees at hilltop” Do this only to allow nature to remove them – rot, wind throw!

Q. 7 – Option c – selectively clear grove to permit integrated development to include museum, library, retail and residential.

Q.9 – This is ultimately up to P.A.P.A. and cruise industry, not public. Pocket cruise ships are red herring! Those are not a real opportunity as no-few exist on PALNW cruise industry!

Comments or other suggestions:

A clear dichotomy of views have evolved based on age and length of residency in Port Alberni.

-The older the person and the longer their tenure as residents, the worse they see the “community’s problems” but the less they want to see anything change to fix/address them.

-The younger the adult and less time as resident the issues are viewed as not unique and not that bad on relative terms. Yet, this group is in favor of grasping the opportunity to improve our community’s future today!

So the choice is: Does the city remain stagnant by listening to those who are “stuck in the good ol’ days” or capitalize on the energy and enthusiasm of this community’s next generation? If not the latter there will be no next generation, no future!

Uptown & Waterfront Redevelopment – City of Port Alberni & Port of Port Alberni

Alternatives Workshop Response Form **LETTERS**

The City and Port Authority are planning together for the future redevelopment of the Uptown Area and portions of the waterfront between the Somass Mill and the former Alberni Plywood Site. At this point in the process, the consultants are considering a range of alternatives, varying from moderate to significant investment. Funding for improvements may be some time in coming. Other than streetworks, the focus of the City and the Port will be to set the right conditions to encourage private or donor investment, as opposed to funding all major projects themselves.

What aspects of the Alternatives do you support? Both the public input received and technical analysis from the Consultants will be submitted to City Council and the Port of Port Alberni as a part of recommendations.

From Helen Weaver:

I would like to add my voice to those who object to the building of condominiums on the treed property at the bottom of Argyle Street, overlooking the harbour.

I am not against development in the Valley and recognize that attractive condominiums will be an asset to this part of the City. However, there is virtually no green space and no trees anywhere along our harbour, and certainly no where that permits a long distance view of the water. Properly managed, this small park could end up being quite an attraction and place of enjoyment of the people living and working in the area, shoppers and other visitors.

Rather than removing the lovely established trees and eliminating the shade and protection from the weather they provide for any new construction there, why not incorporate the park into the development? A creatively designed condo complex on the site could retain much of the green space and trees while enhancing the park to make it even more appealing and useable for all.

From Jean:

Scott, I found the consultant's proposal interesting. Congratulations – you had a good attendance. I have filled out the questionnaire, but from the Museum/heritage perspective I want to mention a couple of things to you specifically. Basically, improving access and linkages between the Maritime Discovery Centre complex and other amenities along the waterfront is of primary importance to us. While the historic Port Alberni Railway Station does not have the same location challenges, any improvements to access and visibility is helpful to us. So generally any plans that generate visual interest in, and visitation to, the harbour area is positive to us. I am also interested in drawing visitors up Argyle Street, and there may be some alignment here with the Heritage Register and the potential development of walking tours which would likely emphasize that corridor and adjacent areas.

I am very much in favour of the use of industrial or marine artifacts in public spaces and I was pleased that the consultants recognize the value of using themes intrinsic to the community, not an imported theme as some do. I think in his presentation he mentioned something about public art. It would be cool to have not only real artifacts, but some form of commissioned public art inspired by or incorporating those industrial/marine artifacts. Of course the issue with displaying real artifacts is we have to select something that is destructible – ie: can be altered to make it safe for unsupervised public access, can accept vandalism, is large and interesting. Etc.Etc. – not always as easy as it sounds, but we are in favour of working toward this goal.

Look forward to hearing more on this proposal.

From Tim Towle:

Sir:

I have just viewed the re-development plans for the Uptown and Waterfront and there are many exciting options – however there appears to be no revisions that are disabled friendly. While we are making improvements let's benefit all people – residents and visitors alike. Imagine Port Alberni as being a place where families can be assured that all members can have a safe and enjoyable holiday and the "hometowners" can enjoy all this town has to offer. With the baby boomers aging it only makes sense to make changes that are inclusive.

Some suggestions:

- Make space on the roads or sidewalks for scooters, motorized wheelchairs, etc.
- Have handrails and seating on the steeper hills for those who walk with aids.
- Have 'tweeters' at intersections for the visually impaired.
- Be sure that all venues (including marine) can be accessed: ie-ramps etc., boats with lifts etc.
- Subsidize (if necessary) accessible transit and shuttle busses.
- Ensure that the shops are accessible by eliminating hydraulic doors, narrow aisles, difficult entrances, etc.
- Provide accessible public washrooms.
- Ensure that service dogs are welcome at all venues.
- Ramp all sidewalks and provide cutaways in parking areas, with adequate space for vans and ramps.
- Re-institute impressed street and block identification in sidewalks at intersections and/or install recorded information that automatically informs pedestrian when the use the "press to cross" option, to aid the visually impaired.

Port Alberni could be unparalleled as an accessible city. Well advertised as such, this community could be the "The Place to See" – a veritable "Disneyland" for people with disabilities. At the very least it would make the motto "City with a Heart" even more meaningful. And on a purely commercial note, it would be good business – there are many people with both disabilities and money that would love to find a place such as we have to live or more importantly vacation in!

As a person with disabilities who is a life long resident of Port Alberni, I have often had to forgo even such simple activities as shopping due to the difficulty in getting around. I appreciate greatly those sidewalks that are cut down, but have found myself stranded on sidewalks, unable to get off!

Thank you for the opportunity to share my views.

From Unknown:

Dear Scott:

I am writing this letter as I was unable to find the forms on the City of Port Alberni website.

I do agree that we must re-develop the Waterfront site but I do not agree that there should be any public funds or resources to re-develop the Uptown core. As you are aware there was a major re-development done in the past. The City of Port Alberni installed new lighting, parking, crosswalks, benches, etc. These improvements are still in good shape. As even our public employees including our City Council must admit this project did not secure the success of the Uptown area. This area is now host of vagrants which can be very annoying and sometimes dangerous. I have on a Saturday afternoon been confronted by some of the vagrants and they are very insulting and frightening. It is so bad that I make it a practice of not going to the ATM after 6:00pm.

As you are aware the private sector has chosen the Johnston corridor and the Redford/10th Avenue area as the locations to develop their private enterprise. This choice was made based on research that they did based on factors which influence the economics of financial flow. The private sector has limited funds to invest and they must locate in the area which will give them the best return on their investment. It always seems that the Public Sector misses the fact that there must be a return on investment. This is probably a

result of the sector feeling that have unlimited funds and limited accountability. The amount of resources whether it be dollars or manpower spent on the two Uptown city blocks from Argyle south compared to what is spent on the whole Johnston corridor is very disappointing. The City must realize that if it were not for the investment the Public sector has made that road would look very sad. Wake up Port Alberni, that is the main corridor that visitors and tourists take. I would guess that 80% of first time visitors do not even drive to 3rd Avenue.

The Public sector must wake up and focus on the real positive economic areas like the Private sector has and spend the energy and funds to improve those areas. Start serving the taxpayers and make the people and businesses in the Uptown area fund their own improvements like the people and businesses on the Johnston corridor and Redford area have done!

I do believe and support re-development of the Waterfront areas with the Public sector being actively involved in the coordination and promotion phases. I do not however support major investment of Public funds.

Thanks.

Appendix C

Contact with First Nations

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March 22, 2006

Mr. Les Sam, Chief Councilor,
Tseshah First Nation,
P.O. Box 1218,
Port Alberni BC V9Y 7M1

Dear Mr. Sam,

Re: Uptown and Waterfront Redevelopment Study, Port Alberni

On behalf of the City of Port Alberni and of the Port Alberni Port Authority we are writing to formally seek the participation of the Tseshah First Nation in an uptown and waterfront redevelopment study. The Lanarc Consulting Ltd. team (consists of planners, landscape architects, transportation and civil engineers) has recently been selected by the City of Port Alberni and the Port Authority to conduct the study. The study will focus on four specific areas:

- Third Avenue, between Dunbar and Mar St.
- Argyle Street, between Fourth Avenue and the Harbour Quay.
- The waterfront between Somass Mill and APD Mill.
- The former plywood mill site at the western city boundary.

For the port waterfront areas the purpose of the study is to review and update a portion of the 'Shoreline Master Plan Review' that was completed in 2000. Under the *Canada Marine Act* the Port Authority is required to maintain a conceptual land use plan. Lanarc consultants will be making recommendations and outlining principles to assist future decision making with respect to development projects, and guidelines for land use. For example, the consulting team will identify the types of physical improvements that would encourage the development of Port Alberni as a cruise ship market. In addition, the consultants will also consider ways of increasing the attractiveness of the uptown area of Port Alberni for both tourists and residents alike in order to maintain or improve the viability of the area for business investment. For the City owned former plywood site and adjacent waterfront, the objective is to create two site potential studies for redevelopment – one that respects an existing covenant prohibiting overnight accommodation on the site, and another that shows the potential if the covenant were lifted.

The final recommendations would be made to City Council and the Port Authority in early fall 2006. Decisions on the consultant's land use planning recommendations rest with City Council and the Port Authority for their respective jurisdictions.

Given your First Nation's interests we are requesting a meeting with the Tseshahat to learn about your land use goals and how your interests could be considered in the development of the consultant's recommendations. From now until the end of June, the consultants will also be listening to issues and ideas, developing alternatives, and forming draft recommendations. In addition to specific First Nations consultations, we would warmly welcome your participation in the public consultation steps associated with this project. Three events are currently being scheduled. We would encourage you to have representation at these meetings:

1. **Executive Meeting: March 30, 2006**, Council Committee Room at City Hall, 12:00 noon to 2:00 p.m. Purpose: to introduce the project to leaders of stakeholder groups, and to discuss relevant issues and ideas. We would be honored if you could attend personally, or send a representative of your First Nation to this executive event.
2. **Issues and Ideas Workshop: April 20, 2006**, Echo Centre, 7:30 to 9:30 p.m. Purpose: to involve the broader memberships of stakeholder groups, and the general public, in a discussion about issues and ideas for improvements to the planning area. We ask you to encourage attendance by interested First Nations representatives at this event.
3. **Alternatives Workshop: May 24, 2006**, Echo Centre, 7:30 to 9:30 p.m. Purpose: to review and refine alternatives for improvements. This meeting will be open to memberships of stakeholder groups, and the general public. Again, we ask you to encourage attendance by interested First Nations representatives at this event.

Thank you in advance for your interest and cooperation.

Sincerely,

PORT ALBERNI PORT AUTHORITY


Chairman

CITY OF PORT ALBERNI


Mayor

cc Steve Conway, Executive Director – Tseshahat
Ken Watson, City Manager
Darryl Anderson, CEO, Port Authority
Scott Smith, City Planner



March 22, 2006

Ms. Judith Sayers, Chief Councilor,
Hupacasath First Nation,
P.O. Box 211,
Port Alberni BC V9Y 7M7

Dear Ms. Sayers,

Re: Uptown and Waterfront Redevelopment Study, Port Alberni

On behalf of the City of Port Alberni and of the Port Alberni Port Authority we are writing to formally seek the participation of the Hupacasath First Nation in an uptown and waterfront redevelopment study. The Lanarc Consulting Ltd. team (consists of planners, landscape architects, transportation and civil engineers) has recently been selected by the City of Port Alberni and the Port Authority to conduct the study. The study will focus on four specific areas:

- Third Avenue, between Dunbar and Mar St.
- Argyle Street, between Fourth Avenue and the Harbour Quay.
- The waterfront between Somass Mill and APD Mill.
- The former plywood mill site at the western city boundary.

For the port waterfront areas the purpose of the study is to review and update a portion of the 'Shoreline Master Plan Review' that was completed in 2000. Under the *Canada Marine Act* the Port Authority is required to maintain a conceptual land use plan. Lanarc consultants will be making recommendations and outlining principles to assist future decision making with respect to development projects, and guidelines for land use. For example, the consulting team will identify the types of physical improvements that would encourage the development of Port Alberni as a cruise ship market. In addition, the consultants will also consider ways of increasing the attractiveness of the uptown area of Port Alberni for both tourists and residents alike in order to maintain or improve the viability of the area for business investment. For the City owned former plywood site and adjacent waterfront, the objective is to create two site potential studies for redevelopment – one that respects an existing covenant prohibiting overnight accommodation on the site, and another that shows the potential if the covenant were lifted.

The final recommendations would be made to City Council and the Port Authority in early fall 2006. Decisions on the consultant's land use planning recommendations rest with City Council and the Port Authority for their respective jurisdictions.

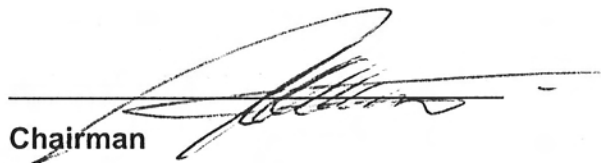
Given your First Nation's interests we are requesting a meeting with the Hupacasath to learn about your land use goals and how your interests could be considered in the development of the consultant's recommendations. From now until the end of June, the consultants will also be listening to issues and ideas, developing alternatives, and forming draft recommendations. In addition to specific First Nations consultations, we would warmly welcome your participation in the public consultation steps associated with this project. Three events are currently being scheduled. We would encourage you to have representation at these meetings:

1. **Executive Meeting: March 30, 2006**, Council Committee Room at City Hall, 12:00 noon to 2:00 p.m. Purpose: to introduce the project to leaders of stakeholder groups, and to discuss relevant issues and ideas. We would be honored if you could attend personally, or send a representative of your First Nation to this executive event.
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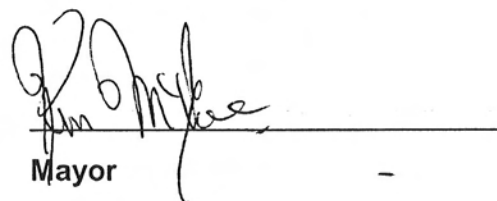
Thank you in advance for your interest and cooperation.

Sincerely,

PORT ALBERNI PORT AUTHORITY


Chairman

CITY OF PORT ALBERNI


Mayor

cc Trevor Jones, Executive Director, Hupacasath
Ken Watson, City Manager
Darryl Anderson, CEO, Port Authority
Scott Smith, City Planner

Appendix D

Capital Cost Estimates for Streetscape Improvements

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Table D-1 Capital Cost Estimates for Streetscape Improvements
ORDER OF MAGNITUDE BUDGET ESTIMATE

DATE Jan-07
Lanarc Consultants Ltd.

Argyle Streetscape Improvements				
ITEM	UNIT	QUANTITY	UNIT-COST	TOTALS
Pedestrian scale luminaire lights c/w banner / hanging basket arms / dual electrical receptacle / drip irrigation sleeve	each	80	\$6,000.00	\$480,000.00
Electrical supply - lighting	each	2	\$1,500.00	\$3,000.00
Tree Location - single receptacle	each	56	\$400.00	\$22,400.00
SUBTOTAL, LIGHTING				\$505,400.00
200mm Dia. Watermain	l.m	380	\$150.00	\$57,000.00
250mm Dia. Watermain	l.m	540	\$200.00	\$108,000.00
200mm Gate valves	each	13	\$800.00	\$10,400.00
250mm Gate valves	each	12	\$1,200.00	\$14,400.00
Re & re existing water services	each	32	\$2,000.00	\$64,000.00
Tie-in to existing mains	each	13	\$5,000.00	\$65,000.00
Hydrants	each	8	\$3,000.00	\$24,000.00
SUBTOTAL, WATERWORKS				\$342,800.00
Re & re existing catch basins	each	15	\$1,500.00	\$22,500.00
Supply and install new catch basins	each	15	\$2,000.00	\$30,000.00
SUBTOTAL, STORM DRAIN WORKS				\$52,500.00
Flush Concrete Band - grade beam	l.m.	700	\$25.00	\$17,500.00
Line painting	l.m.	1500	\$2.00	\$3,000.00
Grind existing asphalt	sq.m.	12000	\$10.00	\$120,000.00
Place asphalt	sq.m.	12000	\$25.00	\$300,000.00
New curb and gutter	l.m.	1700	\$60.00	\$102,000.00
SUBTOTAL, ROADWORKS				\$542,500.00
Solar powered signalized pedestrian crosswalk at Harbour Road	each	2	\$15,000.00	\$30,000.00
Road Crossings @ lanes	each	4	\$10,000.00	\$40,000.00
Undergrounding Utilities on Argyle	l.m.	550	\$300.00	\$165,000.00
Allowance for Telus, Shaw,	allow	1	\$200,000.00	\$200,000.00
SUBTOTAL, WIRING (ALLOWANCE SUBJECT TO CONFIRMATION)				\$435,000.00
Irrigation water supply MH/RPBP/WM	each	1	\$3,500.00	\$3,500.00
irrigation	sq.m.	3200	\$8.50	\$27,200.00
Conduit	l.m.	30	\$19.00	\$570.00
Electrical supply - irrigation	1 m.	1	\$750.00	\$750.00
SUBTOTAL, IRRIGATION				\$32,020.00
Broom finish concrete	sq.m.	6400	\$45.00	\$288,000.00
Unit paving at crosswalks	sq.m.	1000	\$65.00	\$65,000.00
Benches, bike racks	each	8	\$1,500.00	\$12,000.00
Waste receptacles	each	10	\$1,500.00	\$15,000.00
Drinking Fountain	each	1	\$3,000.00	\$3,000.00
Signage system	free standing, allow	1	\$25,000.00	\$25,000.00
Ledgestone clad planter (2m. X 5m. rectangular)	each, installed	30	\$1,800.00	\$54,000.00
Sculptural feature, basic (could be 5X as much)	allowance	5	\$10,000.00	\$50,000.00
SUB-TOTAL, HARD LANDSCAPE				\$512,000.00

Growing Medium for Tree, Shrub planting	cu.m.	1120	\$56.50	\$63,280.00
Landscape Boulders 500mm	each	1	\$135.00	\$135.00
Fine grading & soil placement	sq.m.	325	\$1.50	\$487.50
Shrubs, #1, 300mm soil depth	sq.m.	40	\$27.00	\$1,080.00
Shrubs, #2, 300mm soil depth	sq.m.	160	\$35.00	\$5,600.00
Deciduous trees, 6cm cal.	each	29	\$500.00	\$14,500.00
Evergreen / Coniferous trees, 4m height	each	12	\$2,000.00	\$24,000.00
Bark mulch / Compost in place	cu.m.	24	\$56.00	\$1,344.00
establishment landscape maintenance (depends on area)	allowance	1	\$16,000.00	\$16,000.00
SUB-TOTAL, SOFT LANDSCAPE				\$126,426.50

SUB-TOTAL, CONSTRUCTED WORK				\$2,548,646.50
Consultants' fees and administration	allowance	1	\$509,729.30	\$509,729.30
SUB-TOTAL, IMPLEMENTATION FEES				\$509,729.30

SUB-TOTAL, PROJECT COSTS				\$3,058,375.80
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CONTINENCY @ 30%				\$917,512.74
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ORDER of MAGNITUDE ESTIMATE (Note: Accuracy is +/-30%)				\$3,975,888.54
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Actual costs can vary widely depending on industry labour and material availability

Note: Figures represent 2007 dollars, with no GST assuming municipal project.

Quantities are based on Uptown Waterfront Study Report prepared by Lanarc Consultants Ltd. In January, 2007.

3rd Ave. Renovations and Improvements

ITEM	UNIT	QUANTITY	UNIT-COST	TOTALS
Replacement luminaire lights c/w banner / hanging basket arms / dual electrical receptacle / drip irrigation sleeve	each	19	\$6,000.00	\$114,000.00
Tree Location - single receptacle	each	24	\$400.00	\$9,600.00
SUBTOTAL, LIGHTING				\$123,600.00
Median Curbing	l.m.	475	\$50.00	\$23,750.00
Median Nose Paving	sq.m.	100	\$65.00	\$6,500.00
Line painting	l.m.	180	\$2.00	\$360.00
SUBTOTAL, ROADWORKS				\$30,610.00
Unit paving at crosswalks	sq.m.	500	\$65.00	\$32,500.00
Benches, bike racks	each	17	\$1,500.00	\$25,500.00
Waste receptacles	each	10	\$1,500.00	\$15,000.00
Remove cap, repair stone planters	allow	22	\$1,000.00	\$22,000.00
Signage system	free standing, allow	1	\$25,000.00	\$25,000.00
SUB-TOTAL, HARD LANDSCAPE				\$120,000.00
Shrubs, #1, 300mm soil depth	sq.m.	50	\$27.00	\$1,350.00
Shrubs, #2, 300mm soil depth	sq.m.	100	\$35.00	\$3,500.00
Accent shrub/small tree	each	2	\$125.00	\$250.00
Deciduous trees, 6cm cal.	each	56	\$500.00	\$28,000.00
Bark mulch / compost in place	cu.m.	24	\$56.00	\$1,344.00
establishment landscape maintenance (depends on area)	allowance	1	\$8,000.00	\$8,000.00
SUB-TOTAL, SOFT LANDSCAPE				\$42,444.00
SUB-TOTAL, CONSTRUCTED WORK				\$316,654.00
Consultants' fees and administration	allowance	1	\$63,330.80	\$63,330.80
SUB-TOTAL, IMPLEMENTATION FEES				\$63,330.80
SUB-TOTAL, PROJECT COSTS				\$379,984.80
CONTINENCY @ 30%				\$113,995.44
ORDER of MAGNITUDE ESTIMATE (Note: Accuracy is +/-30%)				\$493,980.24

Actual costs can vary widely depending on industry labour and material availability

Note: Figures represent 2007 dollars, with no GST assuming municipal project.

Quantities are based on Uptown Waterfront Study Report prepared by Lanarc Consultants Ltd. In January, 2007.

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Appendix E

Slide Show Summary of Recommendations

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Final Recommendations

Presentation on February 15, 2007

Presentation of Final Recommendations
Informal Discussion



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Public Input

- March 30/06: Introduction Meeting
- April 20/06: Issues and Ideas Workshop
- Meetings with Tseshaht & Hupacaseth First Nations.
- June 28/06: Alternatives Workshop
- Workshop with Port Authority Board
- Workshop with City / Port Staff
- Informal Meeting with Western Forest Products
- September 29/06: Presentation of Draft Recommendations to Council, Board and staff, who provided input.
- February 15/07: Presentation of Final Recommendations

Input substantially influenced the recommendations. All public input is recorded in the report appendices.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Purpose

- How can the uptown and waterfront be better connected?
- What physical improvements will increase the attractiveness of the study area for tourists (cruise ships and land based) and residents?
- What are candidate land and water uses for vacant areas of the waterfront?

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



The Study Area

- Third Ave, between Dunbar and Mar St.
- Argyle Street, between 5th & Harbour Quay
- The waterfront between Somass Mill and APD mill, and water-based uses through to Coulson's Manufacturing.
- The former plywood mill site.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Planning Process

Four Steps

1. Issue and Idea Identification
2. Evaluation of Options
3. Draft Recommendations
4. Final Recommendations

Complete January 31, 2007

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



The Alternatives Considered

Option A: Low to Moderate Public Investment

Option B: High Public Investment

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Recommendations

An Adaptive Management Strategy

- Recognize changing conditions.
- Adapt timing or priorities based on these changes.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



1. Argyle Infill Development

Encourage redevelopment of vacant city properties - to mixed retail / office / residential



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Key Principles

- Industrial reinvestment is important to the regional economy.
- At the same time, residential / tourism development within walking distance (400m) is important to Uptown.
- The City and Port need to carefully balance their image as friendly to both industrial and residential / tourism development.
- Implementation will rely on small-scale public investment that facilitates large scale private investment.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



2. Harbour View Village

Use air space over Harbour Road to reunite waterfront and upland.

Retain the core of existing fir trees at the hilltop.

Improve the central treed area, parking and street frontage as a high amenity public area and parking.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Nine Priority Projects

- Argyle Infill Development
- Harbour View Village
- Industrial Re-investment in the Plywood Site
- Streetscape Improvements
- Fishermen's Harbour Improvements
- Uptown Village
- Harbour Quay Improvements
- Cruise Ship Infrastructure
- Harbour Shuttle

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

Provide under building parking.

Maintain Harbour Road and Railway access to Port.

Develop the water lot as a marina.

Marine lift access is an option (may not be required).

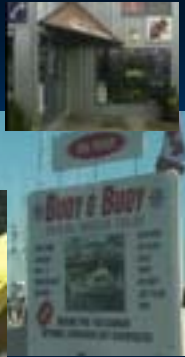


Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

Waterfront commercial for tours and small craft rentals, dive shop?



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village



- boat ramp is replaced by a marine lift – and/or a boat ramp at the plywood site.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

Keep west point of Harbour Quay an open viewpoint.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

- add a waterfront walk to connect through the Esso site.
- improve the old Esso waterfront for public use.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

Waterfront walk fronts retail with parking behind and housing above.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village

New marina floats – both for condo owner boats and for special events / pocket cruises / tall ships, etc.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Plywood Site

Goals:

- to encourage economic development and jobs for PA.
- to increase public waterfront access.
- to co-operate with neighbours – industrial and First Nations.
- to make better use of this under-utilized resource.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Harbour View Village – Backup Plan



- limited development on the north edge of the property.
- affecting up to 10 trees (retail with residential over)
- limited residential development on the south edge of the property – affecting up to 3 trees

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Plywood Site

- assemble, market and sell land for new heavy industry.
- may involve co-operation with A.P.D. and overlap with the A.P.D. site.
- upper, waterfront and marine areas would all be considered to encourage industry investment.
- at the time of sale, negotiations could occur about inclusion of waterfront viewpoints and localized public access.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



3. Industrial Re-investment at the Plywood Site

Partner with WFP.
Provide public access
to viewpoint.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Potential Heavy Industrial Uses

Offshore Vessel Support Base
u similar to Port aux Basques, Newfoundland



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Potential Heavy Industrial Uses

- Wood Products Manufacturing Facility
- softwood lumber, REMAN of KD hemlock
 - structural panels (OSB or plywood)
 - medium density fibreboard
 - specialty sawmill



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Potential Heavy Industrial Uses

- Shipyard with Marine Railway & Turntable
- like Point Hope Shipyard in Victoria



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Potential Heavy Industrial Uses

- Shipyard with a Vessel Lift
- handle vessels with a displacement of >3000 tonnes.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Industrial Re-investment at the Plywood Site – Backup Plan



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Potential Heavy Industrial Uses

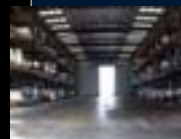
- Shipyard with a Vessel Lift
- handle vessels with a displacement of >3000 tonnes.
 - industrial or mega-pleasure craft.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Boat storage buildings offer attractive protected storage for boats up to about 35 feet long. Boat owner maintenance costs are reduced and the environment is protected from various emissions such as fuel, oil and anti-fouling boat paints.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



For seniors, this type of storage provides a kind of "boat valet" service. For condominium owners and part-time residents this type of storage is particularly attractive. Scarce water frontage is conserved.

Industrial Re-investment at the Plywood Site – Backup Plan



Mixed Use Development
with Noise Mitigation.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Streetscape Improvements



Goal: to increase pedestrian
comfort and safety, and
encourage walking to Argyle
and 3rd Ave. retail areas.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Green Shores

- use 'green shore' techniques as a part of waterfront
development.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle at Harbour Road



Goal: to maintain truck
movements, and allow for the
future Harbour Road
upgrade.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



4. Streetscape Improvements

Rebuild Argyle to
provide a better
pedestrian
connection between
Waterfront and
Uptown



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle at Harbour Road



- install improved materials and
curb bulges to ease pedestrian
crossing of the railway tracks.

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle at Harbour Road

- install pedestrian-activated signals.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area

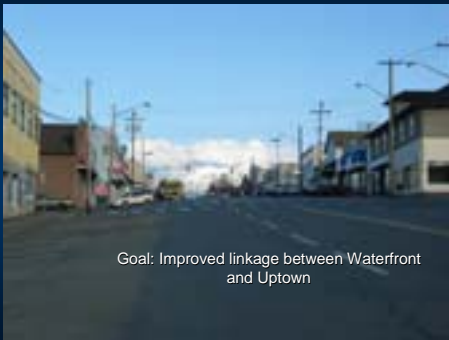


New and wider sidewalks,
add street trees
(entire length)

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area



Goal: Improved linkage between Waterfront
and Uptown

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area



- two lanes for vehicles
- widened sidewalks
- feature areas for interpretive
displays / attractions

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area



Underground O/H Wires

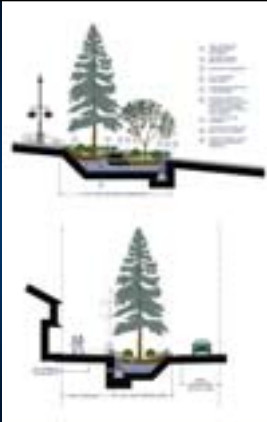
Add curb bulges at
intersections

Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area

-Potential rain gardens at curb bulges to improve stormwater quality.
-Would be subject to senior government assistance for funding.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



3rd Ave. Area



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Argyle Area

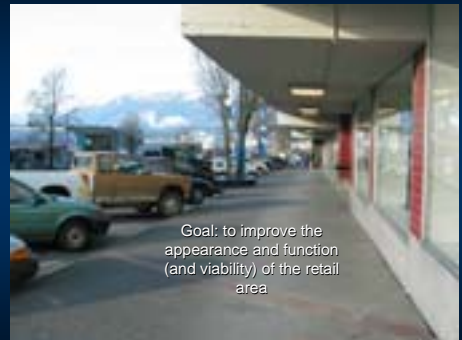
Goals: - showcase an 'adaptable' local theme
- create a series of 'attractions' to pull tourists up Argyle to 3rd Ave.



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



3rd Ave. Area



Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



Streetscape Improvements

Improve streetscape on Third Ave, including medians at Dunbar Hill.

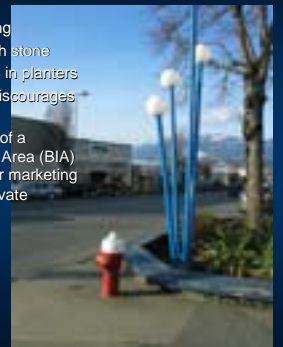


Port Alberni
Uptown &
Waterfront
Redevelopment
Strategy



3rd Ave. Area

- new decorative lighting
- replace wood cap with stone
- renew shrub / flowers in planters
- add new seating that discourages loitering
- encourage formation of a Business Improvement Area (BIA) or other organization for marketing and improvement of private properties.



Port Alberni
Uptown &
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Strategy



3rd Ave. Area



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Redevelopment
Strategy



3rd Ave. Area



- repair sidewalks where required

Port Alberni
Uptown &
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Strategy



3rd Ave. Area

- improve street signage and wayfinding



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5. Fishermen's Harbour

A 20m width along the south side of the Somass Mill would be converted to public pay parking with waterfront walk. The Wharfinger residence moves to a new, additional dockhead location.



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Waterfront
Redevelopment
Strategy



3rd Ave. Area



- repair crosswalk brick paving

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Strategy



Fishermen's Harbour



Land-based Boardwalk and parking on south side of Somass Mill site to a new dockhead

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Strategy



Fishermen's Harbour

An overlook of the harbour activity accessed by a float



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Fishermen's Harbour

- New floats and protection of existing (e.g. foot of Argyle)
- Better truck access and turnaround at dockhead.
- Better function for fish off-loading by large and small boats.



Port Alberni
Uptown &
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Strategy



Fishermen's Harbour

Reserve wharfrage and provide floats for larger vessels



"Pocket cruise" ships like the Sea Lion attract passengers who want to learn about the environment, wildlife and natural scenery. To be a gateway to the west coast of Vancouver Island, Port Alberni may need berthing spaces for such vessels in addition to the MV Lady Rose or MV Frances Barkley.

Port Alberni
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Strategy



6. Uptown Village

Ground Level



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Waterfront
Redevelopment
Strategy



Fishermen's Harbour

Provide a sheltered location for small boat offloading and sales in Fishermen's Harbour



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Uptown Village

Roof Level

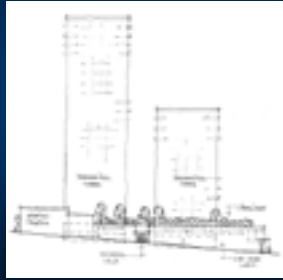


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Strategy



Uptown Village

Elevation Sketch



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Strategy



Harbour Quay Area

Encourage assembly and redevelopment of underutilized land at Harbour Quay north, including Bird Ave.



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Strategy



Uptown Village



Work Live Lofts?

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Strategy



Harbour Quay Area

- provide public sector planning to support land exchange or purchase to create more viable parcels for private redevelopment.



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7. Harbour Quay Area

Goal: Gradual Improvement



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Strategy



8. Large Cruise Ship Infrastructure



This proposal for a cruise berth in Juneau Alaska shows the important elements for serving cruise passengers – except, perhaps, current security needs.

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Large Cruise Ship Infrastructure

- provide attractive pedestrian gate systems for customs clearance needs.
- install landscape and interpretive displays to the welcoming and customs holding area.
- consider a new customs shelter if demand warrants (may be temporary).



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9. Harbour Shuttle



On Land

- a multi-stop narrated shuttle with a special pass

On Water

- a variety of boat / canoe narrated tours

Port Alberni
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Strategy



Large Cruise Ship Infrastructure

- new parking for buses, taxis, etc. extending south from existing.
- attractive fencing/landscape between this transit exchange and the existing port terminal.



Port Alberni
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Redevelopment
Strategy



Implementation Strategy

- Partnerships
- A Role for the Private Sector
- Senior Government Grants
- Phased Improvements
- Financial Strategy in Concept
- Adaptive Management

Port Alberni
Uptown &
Waterfront
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Strategy



Large Cruise Ship Infrastructure



The White Pass Railway in Skagway Alaska is the most important single factor in attracting cruise passengers to this port. The railway attracts passengers and is an important part of the revenues of the cruise lines. Perhaps, Port Alberni has the same potential?

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Strategy



Implementation Strategy

Partnerships

- Key land managers are City, Port and WFP for Harbour View Village and for Plywood Site. A Partnership arrangement should be established – may be one organization or two.
- Small Craft Harbours and the Port may be key partners (with WFP leased land) for improvements at Fishermen's Wharf.
- Uptown Village would require co-operation among landowners and the City.
- Harbour Quay improvements may involve a partnership of City / Port and small land owners.

Port Alberni
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Redevelopment
Strategy



Implementation Strategy

A Role for the Private Sector

- Harbour View redevelopment
- Plywood Site redevelopment
- Uptown Village redevelopment
- Harbour Quay North redevelopment

Would rely on the private sector for the majority of capital funding – development products could exceed \$100M in value.

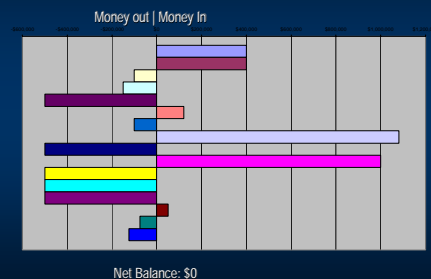
The role of the City and Port is to create the right conditions to encourage this private investment.

Port Alberni
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Waterfront
Redevelopment
Strategy



Implementation Strategy

Financial Strategy in Concept (public funds)



Port Alberni
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Waterfront
Redevelopment
Strategy



Implementation Strategy

Senior Government Grants

- North Island Fund
- Western Economic Diversification
- Municipal Rural Infrastructure Fund
- New Deal Funds
- Spirit Square Fund (BC)
- LocalMotion Fund (BC)
- Greenhouse Gas Reduction Funds (BC and Canada)

All of these funds (and many others) should be aggressively pursued.

Port Alberni
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Strategy



Implementation Strategy: Selected Public Projects

Projects	Cost	Grants	Net
Harbour View Marketing	\$100K		\$100K
Plywood Site Marketing	\$150K		\$150K
Argyle Phase 1	\$1000K	\$500K	\$500K
Uptown Village Feasibility	\$100K		\$100K
Argyle Phase 2	\$1000K	\$500K	\$500K
Argyle Phase 3	\$1000K	\$500K	\$500K
Argyle Phase 4	\$1000K	\$500K	\$500K
3 rd Ave. Improvements	\$500K		\$500K
Harbour Quay C.D. Study	\$75K		\$75K
Cruise Ship Planning	\$125K		\$125K
Totals	\$5,050K	\$2,000K	\$3,050K

Port Alberni
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Strategy



Implementation Strategy

Phased Improvements

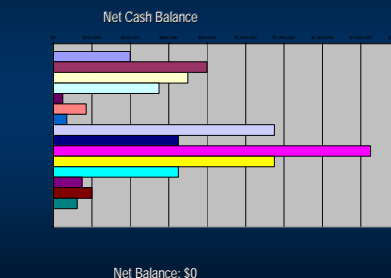
- Major projects should be phased over several years (e.g. this strategy is a 10 year plan).
- Several projects will start detailed planning at once, but major capital costs will be spread over several years.
- Timing of implementation will be influenced by senior government grants.
- Early projects would, ideally, raise funds to be applied to later projects.

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Implementation Strategy

Financial Strategy in Concept (public funds)



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Implementation Strategy

Adaptive Management – adjust the implementation strategy each year based on:

- ❑ Land sale or lease revenue.
- ❑ Private sector interest and market.
- ❑ Senior government grants.
- ❑ Economic opportunities that arise.

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The Strategic Vision

- ❑ The strategy attempts to be visionary, yet practical - a compass that guide the City and Port into an exciting future:
 - ❑ A future where the Waterfront and Uptown are one again.
 - ❑ A future that respects the past.
 - ❑ A future that provides an attractive home where industry, tourism and residents live comfortably and beautifully side by side.



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Appendix F

Map A (separate wall size map)

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